

REPORT TO THE CALGARY PLANNING COMMISSION

DEVELOPMENT PERMIT	ITEM NO: 04	
	FILE NO:	DP2012-4963
	CPC DATE:	2013 April 25

DOWNTOWN
(Ward 7 - Alderman Farrell)



PROPOSAL:
New: Offices, Retail Store, Restaurant - Licensed (2 buildings)

APPLICANT: Dialog Design	OWNER: Brookfield Properties Ltd
MUNICIPAL ADDRESS: 231 – 6 Avenue SW; 250 – 7 Avenue SW; 605 – 1 Street SW	LEGAL DESCRIPTION: (Plan A, Block 30, Lot 1 to 8, 31 to 40 and 0815364 Lot 44 and 41) (Map 15c)
EXISTING LAND USE DISTRICT(S): DC Direct Control District	
AREA OF SITE: 1.35 ha ± (3.34 ac ±)	
CURRENT DEVELOPMENT: Parkade / Vacant land	

ADJACENT DEVELOPMENT:

NORTH: Office – Bow Valley Square (17 storeys SE / 37 storeys SW)

SOUTH: Retail / Office – The Bay / Scotia Centre (6 storeys / 41 storeys)

EAST: Theatre / Office – The Grand / Telus (6 storeys / 17 storeys)

WEST: Office – Intact Place First Canadian Centre (18 storeys / 41 storeys)

DEVELOPMENT SUMMARY			
RULE	BYLAW STANDARD	PROPOSED	RELAXATION
DENSITY	20 FAR	19.54 FAR	None Required
HEIGHT	N/A	247m / 178m	None Required
YARDS (BUILDING SETBACK)	2.134	2.134m	None Required
PARKING	1556 at 50 percent	1252	None Required
EXTERIOR FINISH MATERIALS			
Walls: Silicon Structural Glazing curtain walling			
Roof: Metal			
Windows: Silicon Structural Glazing curtain walling			

SUMMARY OF CIRCULATION REFEREES	
ENVIRONMENTAL MANAGEMENT	ESA was not required
URBAN DESIGN REVIEW PANEL	Comments Provided at APPENDIX IV
COMMUNITY ASSOCIATION Calgary Downtown Association	Supports this application

PLANNING EVALUATION

Introduction

This Development Permit is for the construction of two commercial office towers and a linking pavilion in the Downtown Core with a proposed gross floor area of 256,246 square metres. The east tower would be 57 storeys (247 metres high) and the west tower 41 storeys (178 metres high) incorporating retail and food service uses at grade and the +15 level.

The proposal is for a sequential, two-phase development with the east tower and pavilion/indoor park commencing in Phase 1. Phase 2 is proposed to commence within 5 years of completion of Phase 1

Site Context

The site extends over the entire 200 block between 1 and 2 Street SW, and 6 and 7 Avenues SW. Both 6 and 7 Avenues SW are major transportation corridors for pedestrians, public transport and vehicular traffic, with 7 Avenue SW being a dedicated transit corridor.

The eastern portion of the block is currently being cleared, but previously comprised a surface parking lot, mid rise office/commercial buildings and two storey retail buildings. Three buildings of note were located on the site, the **Herald Building**, the **Herald Mechanical Building** and the **Heagle Building**.

Although included within the City's Inventory of Evaluated Historic Resources, these three buildings did not benefit from statutory designation under the Historic Resources Act. However, in a previous application (DP2008-3576) Brookfield proposed a commemoration of the history of the Herald site and a commemorative plaque, in addition to the retention and integration of the façade of the Heagle building into the new development.

The west portion of the block contains the **Bow Parkade** (formerly the **Bay Parkade**) which was constructed in 1956. Although a utilitarian structure, it was built in the International style and is the oldest multi-story parking structure in Alberta and the second oldest in the country.

This structure is included in The City's Inventory of Evaluated Historic Resources, but does not benefit from statutory designation under the Historic Resources Act.

Land Use District

The project site is designated Direct Control District (Bylaw 28D2013) adopted by Council in March 2013. The Direct Control District allows for a variety of appropriate commercial, institutional and residential uses and establishes the density of a site the application of an incentives system that allows for additional floor area where certain public amenities are provided. A density of 7 FAR (Floor Area Ratio) can be achieved through the provision setbacks, +15 elements and on site open space. The density proposed on the site is 19.54 FAR. This is below the 20 FAR cap defined in the CM-2 District and reflected in the Direct Control District and is considered to be appropriate for this particular downtown location. The bonus incentive standards in both the 'A' and 'B' categories have been used and because the project exceeds 15 FAR, the 'C' incentives as described in the Direct Control District have also been used.

Bonus Incentive	Incentive Area Achieved	F.A.R.
A1 - A3 (Mandatory) Pedestrian Circulation +15 Provisions At-Grade Open Space	91,798m ²	7.00
B1 At-Grade Plaza	44,100m ²	3.36
B2 Space: Other At-Grade	10,103m ²	0.77
B3 Sloped or Terraced Open Space	635m ²	0.05
B5 Outdoor Built Over Space	620m ²	0.05
B6.1 Indoor Park at grade	14,013m ²	1.07
B6.2 Indoor Park at +15 Level	9,490 m ²	0.72
B7+15 Walkway Enhancement	472m ²	0.04
B8 +15 Bridge	5,738m ²	0.44
B9 Escalators	4,560m ²	0.35
B11Improvement to Right-of-Way	3,535m ²	0.27
B12 (a) Sculpture in Public Spaces	13,114m ²	1.00
B12(b) Financial Contribution to Public Art Fund	1,967m ²	0.15
B13 Sites Retaining Heritage Features	3322 m ²	0.25
Sub Total	203,424m²	15.52
C4 At Grade Plaza - Additional	3,690m ²	0.28
C5 Exceptional Design	13,114m ²	1.00
C6 +15 Bridge Refurbishment	3,240m ²	0.25
C7 Heritage Design Feature	13,114m ²	1.00
C8 Heagle Façade Retention	6,557 m ²	0.50
C10 Bicycle Facilities	13,065m ²	1.00
Total	256,204m²	19.55

DENSITY INCENTIVES TO ACHIEVE F.A.R.

Plaza Bonus Incentive

Two plazas are proposed on the block, a large space delineated by tree planting on the south east corner of the block which would benefit from the sunlight, maximized by the restricted height of The Bay and a much smaller treed plaza on the south west corner.

The south west plaza would be fronted, primarily, by a restaurant use that would provide a level of activation, which could be increased through the incorporation of an outdoor patio. Although there is access to the building is at its south west corner, the plaza would function as a meeting space rather than forecourt to the building and public seating proposed around the trees would support the function of this space.

The south east plaza would comprise a large, level, open central section to allow for events, tree planting in front of the building with opportunities for the restaurants within the building to activate the space, block tree planting on the southern edge of the plaza to allow for casual seasonal seating, a restricted and a active kiosk area at the south east corner.

Brookfield has stated that the company has a significant history of actively programming spaces in their global portfolio and that the south west plaza would provide a scale of place to replicate the active arts and events similar to those held in Toronto and New York.

In Calgary, Brookfield states that it currently hosts nearly 500 events in their existing Downtown portfolio and consider that this space would allow the enhancement of that programming.

Indoor Park Bonus Incentive

The indoor park at grade is shown as a lobby space within the pavilion and incorporates the 'Spanish Steps'. At +15 Level, the indoor park comprises 28 'multi-trunk, wispy trees' in a formal double row planting within the ground plane and in line with the column grid. It is surrounded by the +15 walkway, restaurants take-away food service units. The performance requirements of the Direct Control District indicate that the space should be 'intensively landscaped to create a park-like setting which must include seating, and may include playgrounds and performance areas'.

Notwithstanding Brookfield's history of actively programming spaces, Administration considers that the grade level indoor park needs to be connected visually to that on the +15 Level for legibility and continuity. For this reason, a Prior to Release condition has been added requiring additional landscape details for the indoor park. .

+15 Bridges Bonus Incentive

Two +15 Bridges are proposed that would connect the development to the north and the south, completing an important link which will contribute to the vitality and viability of The Core, Scotia Centre and Stephen Avenue Mall.

To the north a new +15 Bridge would connect across 6 Avenue SW to Bow Valley Square. To the south the existing +15 and +30 Bridges between the Bow Parkade and Scotia Centre would be removed and a new +15 Bridge erected.

The refurbishment of the west +15 Bridge to Intact Place would represent an opportunity for an upgrade in terms of its materials, visual appearance and longevity. The maintenance and operation of the Bridge is the responsibility of Intact Place and confirmation of their acceptance of the proposal or transfer of responsibilities would be required.

Escalators Bonus Incentive

The provision of four escalators is proposed, one in each of the towers and two adjacent to the 'Spanish Stairs' in the Pavilion. The intent of providing an incentive for escalators was to move pedestrians efficiently between grade and the +15. As a whole block development, Administration considers it important to have these escalators throughout the block and considers that the necessary easements would ensure vitality and viability of the retail / restaurant uses.

Right-Of-Way Improvement Bonus Incentive

The improvements to the adjacent Right-of-Way comprise the use of granite for the sidewalk and the provision of street trees on 6 Avenue SW and 1 and 2 Streets SW. Street trees cannot be provided on 7 Avenue SW because of the number and placement of utilities. The location of the block within the transit system also creates a requirement for bus shelters and these are required to be provided on the sidewalk at all block faces as part of this bonus incentive.

Sculpture Bonus Incentive

An indicative location has been identified at the east end of the main plaza for a sculpture, however Brookfield considers that this should be determined in association with the artists and the specifics of the art piece to ensure that it has the appropriate impact and does not adversely impact event programming. Brookfield would enter into a legal agreement that would require engagement and collaboration with the City's Public Art Board and the provision of a letter of credit until the sculpture is installed. For these reasons Administration is satisfied that the indicative location is adequate at this stage.

Heritage Bonus Incentive

The restoration of the Heagle Façade was negotiated as part of the bonus initiative package on the previously approved Development Permit for the eastern portion of the site (DP2008-3576) which related to the redevelopment of the east portion of the block.

Administration considers it to remain a valuable incentive, however once removed from the site, its historic context and value would be reduced, with no tangible public benefit would derive from it being privately held and stored. For these reasons, the applicant proposes to relocate and interpret the Heagle façade within the primary circulation core on the central axis of the pavilion.

This would occur in Phase 2 with the construction of the 'Spanish Steps' and the associated retail unit and would provide a positive solution to the re-use of the Heagle façade in a protected location on the site.

Legislation & Policy

The subject parcels have recently been re-designated as Direct Control District (28D2013) which is based on the CM-2 Downtown Business District, but provides for additional bonus incentives. The Direct Control District provides for predominantly commercial development, with a comprehensive system of grade and +15 level amenities to achieve the Floor Area Ratio required.

The Centre City Plan (2007) provides the policy framework for downtown and seeks to enhance it as the primary office location through the provision of high quality public space realm and supporting amenities and services.

The proposal is deemed to be consistent with the Direct Control District and the policies of the Centre City Plan.

Shadow Restrictions

A shadow study provided by the applicant, demonstrates that none of the eight important public areas identified in the CM-2 Business District would be placed more in shadow as a result of the construction of the buildings identified in the proposal during the dates and times set out in the Bylaw.

Site Design

The submitted proposal covers the entire block and comprises a 57 storey tower on the east portion of the block, a 42 storey tower on the west portion of the block and a two storey, glazed linking pavilion between the two.

The main vehicular access would be provided from 6 Avenue SW, together with separate bicycle access and pedestrian access to the office lobbies.

Plazas are proposed at both the south east and south west corners of the block. On the south east, the plaza is significant which would allow for large scale public events and would incorporate a permanent food kiosk as a focal point adjacent to the intersection.

The plaza on the south west corner would be smaller and more intimate with opportunities for outdoor café space for the adjacent use.

The two storey glazed pavilion would provide for commercial uses at both grade and the +15 level, together with an indoor park space. This pavilion would also provide for a north/south +15 skywalk.

Site Lighting Strategy

A microprocessor based lighting control and energy monitoring system would be provided to control all lighting in the building, with occupancy sensors throughout office floors. The lighting would use high efficiency, high colour rendering lamps to mitigate any light pollution.

The lighting concept for the plaza would focus on reinforcing the various zones identified in the plaza plan. The inset area of the plaza and fixed benches along the perimeter would be highlighted with a continuous light source to define the space and provide security. The steps at the west of the plaza would be lit for safety and security and to accent the access to the pavilion. The plaza trees would be up-lit from low wattage/low output, in ground fixtures to accentuate the planting but avoid excessive heat or light pollution.

It is considered by Administration that the site lighting strategy has taken account of both the Centre City Illumination Guidelines and the Bird Friendly Urban Design Guidelines.

Building Design

The 57 storey east tower would provide 52 storey's of office accommodation, 3 mechanical floors (levels 3, 56, 57), 1 retail food service floor (+15 Level) comprising 3 restaurants (67, 48 and 188 square metres) and 1 lobby and retail floor comprising 2 restaurants, one of which would be on 2 levels (216 and 787 square metres).

The two storey pavilion would provide for site servicing from 6 Avenue, washrooms and space for switch gear with service retail (495 square metres and 171 square meters respectively) either side of spine corridor linking the two towers and an indoor park with 14 meter wide 'Spanish Steps' flanked by escalators to provide access to the +15 level. On the +15 Level would be an indoor park, more intensively landscaped than the one at grade and the required walkways for the +15 Skywalk.

The 42 storey west tower would provide 36 storey's of office accommodation, 3 mechanical floors (levels 3, 41, 42), 1 retail food service floor (+15 Level) comprising 7 restaurants (58 1122, 48, 28 and 285 Square metres) and 1 lobby and retail floor comprising a restaurant (338 square metres) and service retail (104 square metres).

The building would be constructed of structural silicone glazing, which would provide a smooth glazed finish. Floor and ceiling voids would be obscured by the use of an angled spandrel panel behind the exterior glass, which would create a gradation of colour over 1 metre and provide an articulation of the façade.

The kiosk set in the plaza would be constructed with a structural glazing curtain wall of clear and translucent glass with avoid filled with a decorative interlayer such that the appearance of the kiosk will change during the day and the inclusion of LED lighting along the base of the kiosk will provide a semi transparent appearance during the evening an night.

The doors and folding canopies would be glass bonded to a metal frame so that when they are closed they would provide a concealed, seamless appearance.

Pedestrian Wind Condition Report

The proposal is supported by a Pedestrian Wind Condition Report prepared by RWDI consulting engineers and scientists. This report concluded that overall, pedestrian wind comfort conditions in areas on and around the site were satisfactory for the intended pedestrian usage and were found to be similar for the existing and proposed configurations.

+15 Connections

Administration considers that the ability to provide connections and a north-south +15 route through the development would be a significant benefit the +15 Skywalk, general pedestrian movement through the city and the vitality and economy of Calgary.

The existing +15 and +30 Bridges that connect to Scotia Center south of the parcel, which are not owned by The City, would be removed and a single connection, at +15 level, constructed in the same location.

The route north across 6 Avenue SW to Bow Valley Square would connect to an existing

easement and supports and provide public access to Scotia Centre, The Core and Stephen Avenue Mall.

The route to the west has an existing +15 Bridge connection from the Bow Parkade to Intact Place (formerly Energy Plaza). The proposal includes a bonus initiative to refurbish this +15 Bridge which would be completed as part of Phase 2 of the proposal.

The site layout, which maximizes the sunlight opportunities for the plaza and places the tower opposite The Lougheed Building, prevents the opportunity for a satisfactory connection to the east.

Administration has assessed the +15 issues and considers the access to the +15, the proposed route through the development and the general design of the +15 Bridges to be appropriate.

Landscaping

Landscaping on the site would be achieved through the two public plazas at the southeast and southwest corners respectively. The landscape design incorporates light and medium grey flamed granite paving which delineates the open area from the tree vault areas. Granite clad seat walls that would be both robust and durable are located along 7 Avenue SW adjacent to the sidewalk and provide casual seating and incorporate a venting system to the underground parkade.

A location towards the eastern edge of the south east plaza has been identified as the location for a sculpture. This sculpture would form part of the bonus incentive package and would be provided through consultation with The City's Public Art Board.

The plazas are intended to be permeable both visually and physically. The planting would be deciduous trees in formal rows that would provide visual interest and shade opportunities while also allowing for natural surveillance. Lighting has been incorporated in the landscape design to contribute to the year round activation of the plaza.

Plaza trees have been identified as paper birch or prairie spire ash with a 75mm caliper. Although the submitted landscape plan shows trees 4.0 metre canopies, this situation will not be the case when first planted, 75mm caliper trees would have canopies of approximately 1.25 - 1.5 metres. However, the proposed tree spacing is considered appropriate and 75mm caliper trees are an ideal size to plant in trenches or underground planters.

The applicant has indicated that it operates the largest, privately funded, free arts and events program in North America, presenting over 400 events, performances and exhibitions annually and the landscape design of the south east plaza has been derived with this in mind

The proposal also includes a retail kiosk within the plaza that would provide the opportunity to activate the plaza on a daily basis. This kiosk would use the same minimalist design form as the office towers. The applicant anticipates that the kiosk would provide an all-day offering and may be associated with one of the building restaurants.

Urban Design Review Panel Comments

UDRP Comments	Applicant Response
<p>The Panel commends the Applicant for a thoughtfully conceived and articulated design concept.</p>	<p>The applicant agrees with this position and will continue to develop the design accordingly.</p>
<p>The Panel notes that the power of the minimalist design rests on the quality of the buildings' interface with the public realm. While the Applicant has carefully and satisfactorily resolved the design of the building envelope above the ground plane, the public interface is largely dependent on the selection of materials and the design of their details where the building envelope meets the ground plane.</p>	<p>The applicant agrees with this position and will continue to develop the design accordingly.</p>
<p>The Panel feels that the indoor park spaces are not adequate in terms of vegetation and landscaping. The panel requests an expanded and comprehensive landscaping plan to enhance the quality of the indoor parks.</p>	<p>The pavilion is conceived as an active, vibrant interior urban piazza modeled on successful examples within the Brookfield portfolio and elsewhere.</p> <p>It is also understood that it must enrich the urban environment during times when there are no events or exhibitions ongoing. A delicate balance needs to be reached between these aspirations.</p> <p>During the many events that will be held there, the movable seating will be either rearranged or removed as appropriate. Free Wi-Fi and ample public seating will be provided which, combined with the rich variety of food and beverage offerings will create a very 'sticky' place.</p>

A full version of the UDRP comments and applicant's responses are included at APPENDIX IV.

Site Access & Traffic

The subject site is bounded by 6 Avenue SW, 7 Avenue SW, 1 Street SW and 2 Street SW. There is currently vehicular access from all the surrounding roads with the exception of 7 Avenue and this proposal would rationalize the accesses, providing two in/out paired accesses to 6 Avenue SW

Site servicing would be accommodated below grade (P2), but access, together with that of the parkade would be via 6 Avenue SW.

Pedestrian access to the east office tower would be available from 6 Avenue SW and the plaza on 7 Avenue SW, to the west tower from 2 Street SW and the smaller plaza on 7 Avenue SW and to the pavilion from both plazas and 7 Avenue SW.

Access to the +15 Skywalk and restaurants would be from both east and west office towers, via escalators, stair or shuttle elevators and the 'Spanish Stair' in the pavilion.

Transportation Impact Assessment (TIA)

The TIA states that at the completion of Phase 1 and 2, the development is forecast to add approximately 9,761 employees. It is estimated that the majority of employees will either walk, bike, or take transit to/from the site.

The site is well served by transit, currently it has forty seven (47) bus routes and two (2) C-Train routes operating within 400 metres. Calgary Transit has indicated that existing bus and train services are adequate for the existing and future needs of the site.

There is adequate pedestrian and cycle connectivity from the site and the sidewalks surrounding the site are in good condition. The development is proposing to include the provision of 580 secure bike lockers with showers available for employees on the P1 level of the parkade that are accessed from the 6 Avenue with separate ramps.

Upon the completion of Phase 1 and 2 the development is forecast to generate 726 AM (morning) peak hour vehicular trips and 655 PM (evening) peak hour vehicular trips. Furthermore, the three existing Bow parkade accesses (one onto 2 Street SW and two onto 6 Avenue SW) will be replaced with two accesses onto 6 Avenue SW.

The intersections of 6 Avenue SW and 1 Street SW and 6 Avenue SW / 2 Street SW are expected to operate in a manner that is generally consistent with existing conditions; there would be increases in queue lengths along 6 Avenue SW, particularly during the PM (evening) peak hour; however, no changes are required as a result of the new site traffic.

Also, outbound queues within the site are expected during the PM (evening) peak hour as vehicles wait for available gaps in the 6 Avenue SW traffic stream.

Parking

The subject parcel is located within the Downtown Restricted Parking Area. The parking requirement set out in Part 10 of the Bylaw is 1 stall per 140m² of net floor area in the Downtown. However the number of stalls provided on site is restricted to 50 percent of the number required for the use under the Restricted Parking Bylaw.

The proposal represents 217,809 square metres of net floor space which would result in a parking requirement of 1556 stalls, reduced to 778 as a result of the 50 percent restriction. The other 778 stalls not provided on site, resulting from the restricted parking would be required as a cash-in-lieu financial contribution.

However, the Direct Control District allows for the replacement of 1038 parking stalls from The Bow Parkade. The actual number of parking stalls proposed is 1252. The difference between the parking requirement and the parking supplied on site will be required to be paid as cash-in-lieu.

These 1252 parking stalls are proposed on 7 full levels covering the full site, with 611 of these provided in Phase 1.

The provision of 611 parking stalls proposed in Phase 1 would exceed the 50 percent Restricted Parking Area rules for the use (919 at 50 percent = 460 stalls required), therefore Condition Number 8 has been included to allow the applicant the flexibility of phasing and secure an appropriate level and type of parking within the parcel. Where the surplus parking stalls are secured for public short stay parking, Administration considers that they would have no adverse impact on the peak hour traffic flows.

Loading

Part 10 of The City's Land Use Bylaw requires 1 loading space per 9.300m² of office space, unless otherwise allowed by the Approving Authority. On this particular project, this would represent 28 loading spaces. The applicant is proposing 22 loading bays and has submitted a loading analysis in support of the application that shows that based on the operation and demand of similar projects within the downtown core, a relaxation in the number of loading stalls could reasonably be supported. This analysis has been reviewed and its conclusions accepted by CPAG.

Community Association Comments

Comments in support of the application attached as APPENDIX II

CONCLUSION:

The proposal is supported for the following reasons:

- The proposal meets the requirements of Part 10 of the Land Use Bylaw 1P2007 and the Policy for the Downtown.
- The site design places the tower on the major vehicular corridor, the plaza on the pedestrian/public transport corridor and allows for an important north/south +15 connection.
- The design of the office tower would make a positive contribution to the Downtown's built form and the bridge link would provide efficient and effective pedestrian flows until such time as internal link can be provided within the block.
- The parking meets the requirements set out in Part 10 of the Bylaw and with access located on 6 Avenue would have no material detrimental impact to traffic flows.

CORPORATE PLANNING APPLICATIONS GROUP RECOMMENDATION: APPROVAL

Recommend that Calgary Planning Commission **APPROVE** the application for New Offices, Retail Store, Restaurant – Licensed (2 buildings) located at 231 – 6 Avenue SW, 250 – 7 Avenue SW and 605 – 1 Street SW, with the following conditions:

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of **EIGHT** (8) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

1. **FIVE** (5) of the plan set(s) shall highlight all of the amendments.
2. **FIVE** (5) detailed written response(s) to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.
3. In addition to the full sized plans requested above, please submit one (1) 11 x 17 complete set of plans for the purpose of the Development Completion Permit (DCP) process.

Please ensure that all plans affected by the revisions are amended accordingly.

2. A clearance letter from Enmax Corporation indicating that all of its issues or concerns have been satisfied must be obtained and submitted to the Development Authority. Please contact Enmax directly.
3. A clearance letter from the owner of Intact Place or Bentall Kennedy LP, indicating that all of its issues and concerns have been satisfied in respect of the west +15 Bridge refurbishment with confirmation of future maintenance responsibilities, must be obtained and submitted to the Development Authority. Please contact Bentall Kennedy LP directly.
4. A clearance letter from the owner of Scotia Centre or Cominar REIT indicating that all of its issues or concerns have been satisfied in respect of the removal of the existing +15 and +30 Bridges across 7 Avenue SW must be obtained and submitted to the Development Authority. Please contact Cominar REIT directly.
5. Provide additional design information for the +15 Bridges including glazing, mullion and transom details at a scale of no less than 1:50, an explanation of how snow and ice build up will be addressed, a dimensioned section, a longitudinal section showing the gradient of the internal slope and a detailed materials list to the written satisfaction of the Approving Authority. The +15 Bridges shall be completed in accordance with these approved details.
6. A Development Agreement shall be executed in conformity with all reports, plans and materials submitted to and approved by the Development Authority, and shall include:
 - A standard +15 Development Agreement with The City to the satisfaction of the City Solicitor, including the delineation of +15 easement areas, schedules and maintenance obligations, as well as details of responsibilities for and construction of all improvements within the adjacent public rights-of-way;
 - If applicable, details with respect to contributions to off-site improvement funds in accordance with Bonus standard C2 of Part 10, Bylaw 1P2007, and

- Details of the sculpture and/or amount to be contributed with respect to the provision of sculpture in accordance with Bonus standard B12 of Part 10, Bylaw 1 P2007.
 - Details in respect of the Heagle Building bonus initiative including:
 - (a) confirmation that the Heagle Building facade has been catalogued, dismantled and securely stored for re-erection in accordance with the consultant's report and discussions with the Senior Heritage Planner;
 - (b) that the salvaged Heagle façade will be re-erected in Phase 2 of the development; and
 - (c) any further assurances required by the Approving Authority.
 - Mapping of the Floor Area Ratio of Phase 1 & 2.
7. Submit contribution to the + 15 fund at the current rate at the time of payment. Please contact +15 Co-ordinator (403) 268 8694 for the current +15 fund rate prior to payment.
 8. Submit details of how the parking supply within the parcel will be restricted to a maximum of 50 percent of the required parking for Phase 1, in addition to those stalls provided in the existing parkade (Bow Parkade). An agreement to the satisfaction of the Approving Authority regarding the management of any surplus parking for public short-stay parking only will be required prior to Development Completion Permit for phase 1.
 9. Provide preliminary details of the proposed historic interpretative features, including locations, scale, content and approach, commemorating the sites listed on Calgary's inventory of historic resources that will be or have been demolished to accommodate the new development. The final design details of historic interpretative features shall be approved by the Senior Heritage Planner in writing and installed prior to the Development Completion Permit being issued or first occupation of any parts thereof whichever is the sooner.
 10. Submit plans that clearly identify the parking stalls within the parcel that will be provided as parking – short term at the completion of Phase 2. These parking stalls shall only be used as parking short-term and shall not be sold or leased to the general public for the purpose of long stay parking in the downtown core.
 11. Submit final design details of the proposed indoor park to be created within the pavilion to the satisfaction of the Approving Authority or reconcile the bonus floor area ratio resulting from the use of this bonus initiative through the satisfactory use of an alternative bonus initiative. The indoor park shall be completed in accordance with these approved details.
 12. Submit final design details of the architectural screen on the north elevation to the satisfaction of the approving Authority. The architectural screen shall be completed in accordance with these approved details.
 13. Submit final design details of the proposed plaza kiosk, including glazing details, to the satisfaction of the approving Authority. The plaza kiosk shall be completed in accordance with these approved details.

14. Surrender Development Permit 2008-3576 in respect of New: Offices, Restaurant – Licensed, Restaurant – Food Service and Retail Store at 605 – 1 Street SW.

Urban Development:

15. Amend the plans to:

Roads

Property Lines and Setbacks

- a. Include details for all non-standard right-of-way elements / improvements (granite, etc.), proposed within the City rights-of-way, on / in the amended plans.

Details shall be prepared, stamped and signed by a qualified professional engineer, and shall coincide with the material / details; which were submitted by the applicant on January 23, 2013 (via email); and the response (required revisions) provided by Roads -Materials and Research on February 19, 2013 (via email).

As per Roads -Materials and Research (email); the following revisions were required (for the non-standard right-of-way improvement details):

- Rigid insulation is required for section of sidewalks less than 3.0 m from the building
- Tie bars shall be designed for curb & concrete slab
- Provide details of 2 way reinforcing
- Show sidewalk slope and provide comments regarding sidewalk pavement structure drainage.
- Indicate the thickness of the existing adjacent roadway
- Evaluate if there is need for pavement structure drainage along 6 Ave
- Please specify type of the gravel base
- Identify what stone type 1 and 2 are

For further details, contact the Roads –Materials and Research Engineer at 403-268-2846

16. Provide confirmation that the recommendations outlined in the “Phase 1 Environmental Site Assessment, 231-6 Avenue SW and 250 7 Avenue SW, Calgary Alberta Lots 1-8, 29-40 (or portions thereof), Block 44, Plan A, Bow Parkade. –Trace Associates, February 6, 2012” have been undertaken. Said information shall be submitted and reviewed; to the satisfaction of the City of Calgary, Environmental and Safety Management Department.
17. Submit a Sanitary Servicing Letter, for review and acceptance, prepared by a qualified professional engineer under seal and permit to practice stamp. The Sanitary Servicing Letter shall identify the type of the development, address of the development, existing and proposed peak sanitary flows. **For further information and details, contact the Leader – Development Approvals in Water Resources at 403-268-4636.**

18. Submit two (2) copies of an Erosion and Sediment Control (ESC) report and drawings to Urban Development, for review and acceptance by Water Resources. If the overall site size is less than 2 hectares (5 acres), only a set of drawings may be required for review. **Ensure, in advance, you contact the Erosion Control Coordinator, Water Resources (403-268-2655)** to discuss report and drawing requirements for sites less than 2 hectares in overall size.

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Agrolgist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

19. A fire flow test is required to be scheduled and funded by the developer. Depending on the results, public mains may be required to be upgraded to provide the required fire protection to the site. **Contact Water Services at 403-268-4015.**
20. **Remit payment (certified cheque)** for the infrastructure upgrades for the Centre City communities, in the amount of **\$1,435,471.40**, to Urban Development. This levy includes both the Centre City Utility Levy approved under the **Centre City Utility Levy Bylaw 38M2009** and an amount approved by Council for community recreation, transportation, parks upgrading, and greenways. The amount identified above is determined by using **\$4,710.00 per meter of site frontage (on avenues only)** for the proposed development (**304.771m**).
21. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100 percent of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Construction of new sidewalks adjacent to development site (all adjacent frontages; as per the proposed treatments)
 - b. Construction of two (2) new driveways along 6 AV SW
 - c. Construction of new wheelchair ramps
 - d. Construction of proposed tree trenches and non-standard tree trench grates
 - e. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel
22. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100 percent of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces.
The payment is non-refundable.

Roads

f. Street lighting upgrading adjacent to the development site (all 4 frontages)

23. Submit three (3) sets of the Development Site Servicing Plan details to the Development Servicing, Urban Development, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/DBA/Documents/urban_development/publications/DSSP2012.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/DBA/Documents/carls/DSSP-CARL.pdf>

24. Consolidate the subject parcels. Submit a copy of the registered plan and certificate of title indicating the consolidation of subject parcels onto a single titled parcel to the Urban Development Generalist.

Transportation:

25. The applicant shall provide a vehicle delivery protocol on the client's company letterhead that outlines the planned management practices of the loading area. Include clarification on how the loading area will be impacted during the phasing of the development.
26. Execute and register on title a Sidewalks Access Easement Agreement over **Plan A; Block 44; Lots 1-40** (Servient Lands) in favour **of 6 Avenue SW, 2 Street SW, 1 Street SW and 7 Avenue SW** (Dominant Lands) for the purpose of pedestrian access (2.134 metre easement area adjacent to the property line, corresponding to the bylawed setback area).

Include the 3.0 metre x 3.0 metre corner cut dimensioned from bylaw setback line at all adjacent intersections in this Agreement. The agreement and registerable access right of way plan shall be to the satisfaction of the Director, Transportation Planning. A standard template for the agreement and an Instruction Document will be provided by the Transportation CPAG Generalist. Submit an original copy of the executed agreement and the certificate of title(s), indicating the agreement is registered on title, for all affected parcels.

27. Amend Drawing 31 to label where the Class 2 stalls are located.
28. Amend Drawing 21 label: 'existing bus zone to be relocated' to: 'Existing bus zone sign to be removed'.
29. The developer is required to provide at their cost a standard 2.7m x 8.7m heated transit shelter on 7th Avenue at the location of the existing bus zone. A deposit of \$125,000.00 to cover the cost of the installation of the transit shelter is required for your development

to proceed. This deposit can be a certified cheque or an irrevocable letter of credit drawn from a Canadian bank made payable to The City of Calgary. Please note that as the applicant you are responsible for 100 percent of the installed cost, and any refund or additional amount owing will be addressed at time of installation.

Concrete foundation and underground electrical servicing is to be provided at the full cost of the developer.

The shelter shall be located 5120mm back of curb to maintain consistent sidewalk dimensions throughout the block. The developer is required to enter into an access agreement to maintain perpetual public access to the shelter.

30. The developer is required to provide at their cost a standard unheated transit shelter on 1st Street SW at the location of the existing bus zone. A deposit of \$10,500.00 to cover the cost of the installation of the transit shelter is required for your development to proceed. This deposit can be a certified cheque or an irrevocable letter of credit drawn from a Canadian bank made payable to The City of Calgary. Please note that as the applicant you are responsible for 100 of the installed cost, and any refund or additional amount owing will be addressed at time of installation.

Parks:

31. Amend the **tree inventory (west) plan** to indicate all existing public trees within 6.0m of the development site. As per the Tree Protection Bylaw, provide correct information concerning the public trees information:

- a. Tree species
- b. Diameter of tree trunk (dbh)
- c. Location of the centre point of the tree trunk
- d. Scaled outline of the tree canopy dripline

Trees along 6 Avenue east of 2 Street SW	Species	Height	Calliper	Dripline /Canopy	To remain or to be removed
1	American Elm	9m	19cm	6m	To be removed
2	American Elm	3m	7cm	2m	To be removed
3	Green Ash	9m	20cm	5m	To be removed
4	Green Ash	5m	7cm	2m	To be removed
5	Green Ash	10m	19cm	6m	To be removed
6	American Elm	7m	17cm	8m	To be removed
7	Green Ash	5m	7cm	2m	To be removed
8	Green Ash	7m	13cm	4m	To be removed

32. The five public trees shown at the corner of 1 Street and 6 Avenue SW (**Tree Inventory (East) Plan**) have previously been removed with authorization from Urban Forestry through BP2012-10016.

Amend the Tree Inventory (East) Plan 14 accordingly.

33. Indicate on the site and landscape plans that the minimum setback requirements of proposed boulevard trees such as streetlights and utilities are met as per Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition). Parks also recommends a minimum of 3m setback requirement between the proposed boulevard trees and the Plus 15 bridges.

Adjust the tree spacing of the proposed trees and amend all relevant plans accordingly.

34. Indicate on the site and landscape plans that the proposed boulevard trees maintain a 4m setback from the loading area of the bus stops. Adjust the tree spacing and/or number of proposed boulevard trees. Amend all relevant plans accordingly.

Permanent Conditions

The following permanent conditions shall apply:

Planning:

35. The development shall be completed in its entirety, in accordance with the approved plans and conditions; any changes to the approved plans (including non completion of the development) shall be submitted for approval to the Development Authority.
36. No changes to the approved plans shall take place unless authorized, in writing, by the Development Authority.
37. Any encroachment of the approved uses within the building into the public easement space must have and maintain in good standing a valid Commercial Use of Public Easement Space Agreement.
38. The properly executed Development Agreement referenced in Condition #6 shall remain in force and on title throughout the life of this development.
39. A lighting system to meet a minimum of 54 LUX with a uniformity ratio of 4:1 on pavement shall be provided within the parkade.
40. Handicapped parking stalls shall be located as shown on the approved plans released with this permit.
41. This approval allows **TWO** (2) phases on the approved plans which shall be completed in sequence. A Development Completion Permit may be issued for each phase. All the road works, landscaping and provisions for garbage collection shown within each phase shall be completed prior to the issuance of a Development Completion Permit for that phase. A parking agreement with respect to excess on-site parking may be required prior to the issuance of the Development Completion Permit. Call Development Inspection Services at 268-5491 to request site inspections for the Development

Completion Permits.

42. Prior to the demolition of the existing parkade structure (Bow Parkade) on Phase 2, the calculation for the payment for cash-in-lieu of parking equivalent will be required for any stalls not provided on site. Where a cash-in-lieu payment is required it shall be paid at the Downtown rate current at the time of payment prior to the commencement of Phase 2, failing which this development permit and any work under it will be suspended until such time the funds are paid. Certified cheques shall be submitted to the File Manager along with the development permit number
43. If construction of the subsequent phase has not commenced within **FIVE** (5) years of the issuance of the most recent Development Completion Permit, a development permit for Phase 1 and the existing parkade structure on Phase 2 shall be required, that reconciles the development on the parcel.
44. The landscaping/planting indicated on the plans hereby approved shall be undertaken in the first available planting season following the practical completion of the development or first occupation of any parts thereof whichever is the sooner. Such landscaping/planting shall thereafter be maintained for the life of the development and any landscaping/planting removed or that become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless otherwise first agreed in writing by the Approving Authority.
45. All areas of soft landscaping shall be provided with an underground sprinkler irrigation system.
46. The parking areas identified on the approved plans, with the exception of the 15 percent identified as parking short-term, shall be for the sole use of building occupants. Parking stalls shall not be sold or leased to the general public for the purpose of long stay parking in the downtown core.

Urban Development:

47. The subject parcels must remain on a single certificate of title for the duration of the development unless subdivision is approved by the subdivision authority.
48. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
 - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
49. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Urban Development. All work performed on public property shall be done in accordance with City standards.
50. Canopies and awnings located within the bylaw setback and/or City road right-of-way shall be removed at the owner's expense within 30 days of the City of Calgary giving

notice, as per the Streets Bylaw 20M88, Section 59.

51. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Urban Development, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.
52. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which was submitted to Water Resources for review and acceptance. Any amendments to the ESC documents must be reviewed and approved by Water Resources in advance by contacting the ESC inspector that reviewed the documents or by contacting the Water Resources Erosion Control Coordinator at 403-268-2655.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control (www.calgary.ca/waterservices/esc). Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

53. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
54. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
55. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.

56. As the development application has not met all minimum standards as identified in either the current City of Calgary standards - "Design Guidelines for Development Permits & Development Site Servicing Plans, Waste & Recycling Section" or the current Waste & Recycling Bylaw, the developer or owner shall be responsible for the management of the waste and recycling on the site. The developer shall dispose, handle, store and collect waste on site pursuant to the approved plans provided by **WOODS BAGOT DIALOG (on file DP2012-4963)**.
57. A Perpetual Maintenance Agreement is to be registered on the development site's land title(s) for the applicant requested non-standard surface element(s), non-standard trench grates, etc. located in the road right-of-way concurrently with the execution of the Indemnification Agreement.
Contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.

Transportation:

58. This development is located along the 7 Avenue LRT corridor. Access to the construction site is required to be on 2 St SW or 6 Av SW and shall not impact train service.
59. Construction of elements in, below or above the 7 Avenue corridor must be completed in accordance with Calgary Transit's right-of-way procedures. All activities in the right-of-way, including sidewalks, requires a permit issued by Calgary Transit. The cost of protection of workers and/or equipment during impeding construction is to be at the full cost of the applicant.
60. This site is adjacent to two Calgary Transit bus zones (southbound on 1 St south of 6 Av SW and westbound on 7 Av east of 2 St SW.) These bus zones shall remain before, during and after construction and will be indicated on all site plans and drawings.
61. Existing curb line dimensions and bulb out at NW corner of 7th Ave and 1st St SW are to be maintained.
62. The City of Calgary has completed LRT duct bank upgrades adjacent to the Herald building property. LRT duct banks sit directly adjacent (south) of the existing Herald building basement (less than 500mm below surface and moving east rises up to be approx 100mm below surface at the intersection of 7th Ave and 1st St SW). This bank continues for the entire length of the block between 1 St SW and 2 St SW.
63. Duct banks adjacent to this site are integral to the safe operation of the LRT system. The applicant is required to coordinate with Calgary Transit throughout all demolition and construction projects of this block to ensure neither is compromised.
64. Before excavation activities begin, Calgary Transit shall be granted access to survey track LRT elevations and ensure that any excavation has not, and will not undermine the track structure on 7 Avenue. Shoring plans and designs should be submitted Calgary Transit for review.
65. The proposed rebuilding of the +15 structure shall be coordinated through Calgary Transit. Construction shall be planned in accordance with scheduled track closures and shall not disrupt regular CTrain service.

66. All loading activity shall be contained entirely on-site.
67. During Phase 1, 264 parking stalls in the Bow Parkade will be decommissioned through either stripe removals, chaining off areas, floor access restrictions or any combination thereof. The total number of parking stalls is not to exceed 1,424 during the phasing of the development as per the submitted traffic impact assessment. Stalls in excess of 1,252 must be designated short stay stalls.
68. The development is to maintain at all times 15 percent of its available parking supply as publicly accessible visitor stalls.
69. At the completion of Phase 1, the existing accesses to the Bow parkade are to remain.
70. At the completion of Phase 2, no direct vehicular access to 2nd Street SW will be permitted. Direct vehicular access to the site will be provided via two driveways onto 6th Avenue SW.

Parks:

71. As indicated by the plans, the removal of existing public trees along property frontage is necessary. The applicant will be required to provide compensation to the City of Calgary for any Public Trees that are removed or damaged. As per the City of Calgary Tree Protection By-law, a letter of authorization to remove public trees is required from Parks Urban Forestry. The applicant is to contact Urban Forestry at 311.

The use of an indemnified contractor to remove the boulevard tree and rootball material is required and at the expense of the applicant.

72. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition). Applicant is to contact the Parks Development Inspector at 403-268-5204.

WOODS BAGOT DIALOG 225 SIXTH

LEGAL DESCRIPTION
LOT 10, DISTRICT 11,
BLOCK 44,
PLANNED AREA

DESIGNED FOR
Development Project
- 07th & 11th Regeneration
2012.03.29

DRAWING TITLE
SITE PLAN

SCALE
1:250

GENERAL NOTES

- ADDED DRIVEWAY FLARES
- DIMENSIONED DRIVEWAY
- THREAT FLARES
- PROPERTY LINE, SETBACK & CORNER CUTS INDICATED
- DIMENSIONED



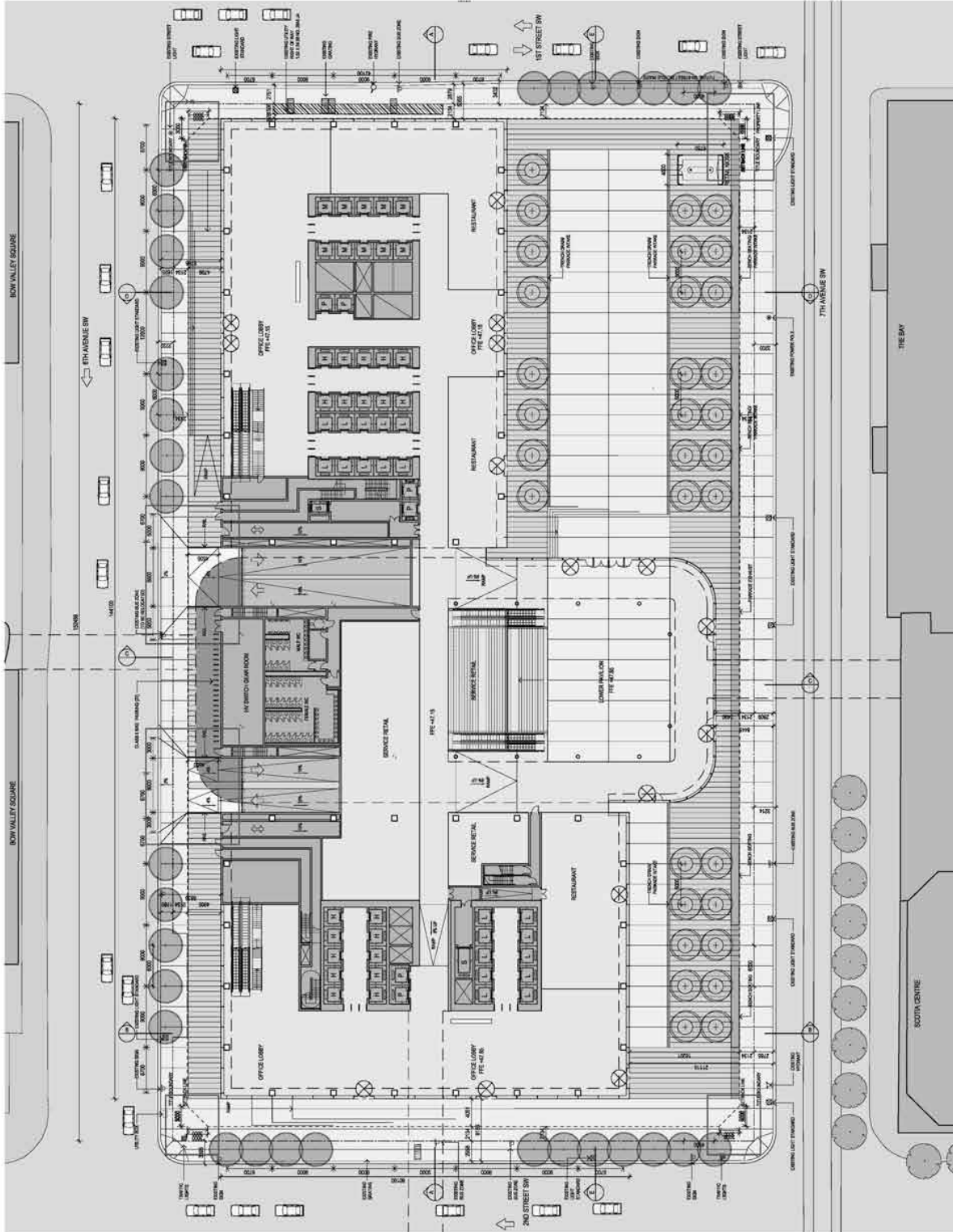
Brookfield

DIALOG
77 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.228.1551
F: 403.228.1554

WOODS BAGOT
76 OXFORD STREET
UNITED KINGDOM
UNIT 105
TEL: +44 (0) 1937 83330
F: +44 (0) 1937 83330

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DIALOG PROJECT NO: 098963-208



WOODS BAGOT DIALOG
225 SIXTH

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 44
PLAZA

SEQUENT FOUR
2012.11.29
Development Permit
2012.03.06

DRAWING TITLE
PHASING STRATEGY

SCALE
ITS
GENERAL NOTES



Brookfield

DIALOG
225 6TH AVENUE SE
CALGARY, AB
T2E 0G5
TEL: 403.228.1651
F: 403.228.0044

WOODS BAGOT
78 OXFORD STREET
UNIT 305
UNITED KINGDOM
LONDON W1D 3SS
TEL: +44 20 7337 8000
F: +44 20 7337 8000

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DIALOG PROJECT NO: 098962206



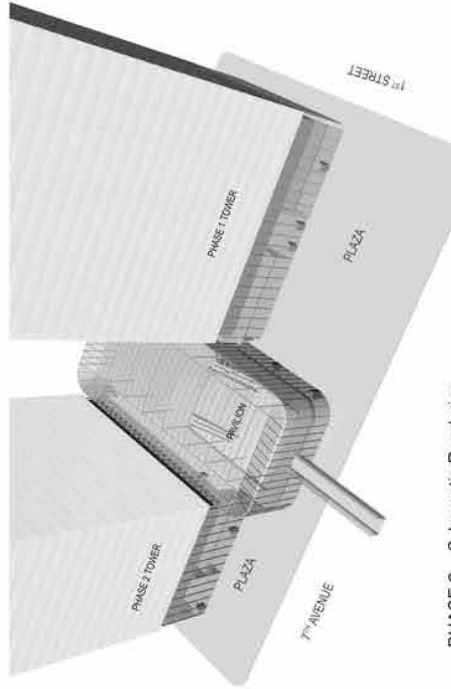
SELECTIVE DEMOLITION - Schematic Rendering

- demolition of +15 / +30 bridge across 7th ave
- demolition of existing parkade ramp
- selective demolition of existing Bow Parkade



CURRENT - Schematic Rendering

- demolition of existing buildings on the eastern half of site underway
- cataloguing of Heagle facade (completed)



PHASE 2 - Schematic Rendering

- western tower and below grade parking
- south west public plaza
- phase 2 completion of Pavilion (public amenity/retail component at ground and +15 level)
- western +15 bridge redeveloped



PHASE 1 - Schematic Rendering

- eastern tower and below grade parking
- south east public plaza
- phase 1 public amenity/retail component - ground floor and +15
- new south +15 bridge
- new ramp between floors internal to Bow Parkade
- temporary stair / elevator core internal to Bow Parkade

WOODS BAGOT DIALOG
225 SIXTH

LEGAL DESCRIPTION
LOT 14 & 15
BLOCK 4E
PLAZA

DESIGN FOR
Development Project
- 076-1-Reg-2006
2012.02.06

DRAWING TITLE
**LANDSCAPE SITE
ILLUSTRATIVE PLAN**

SCALE: 1"=20'-0"
GENERAL NOTES:



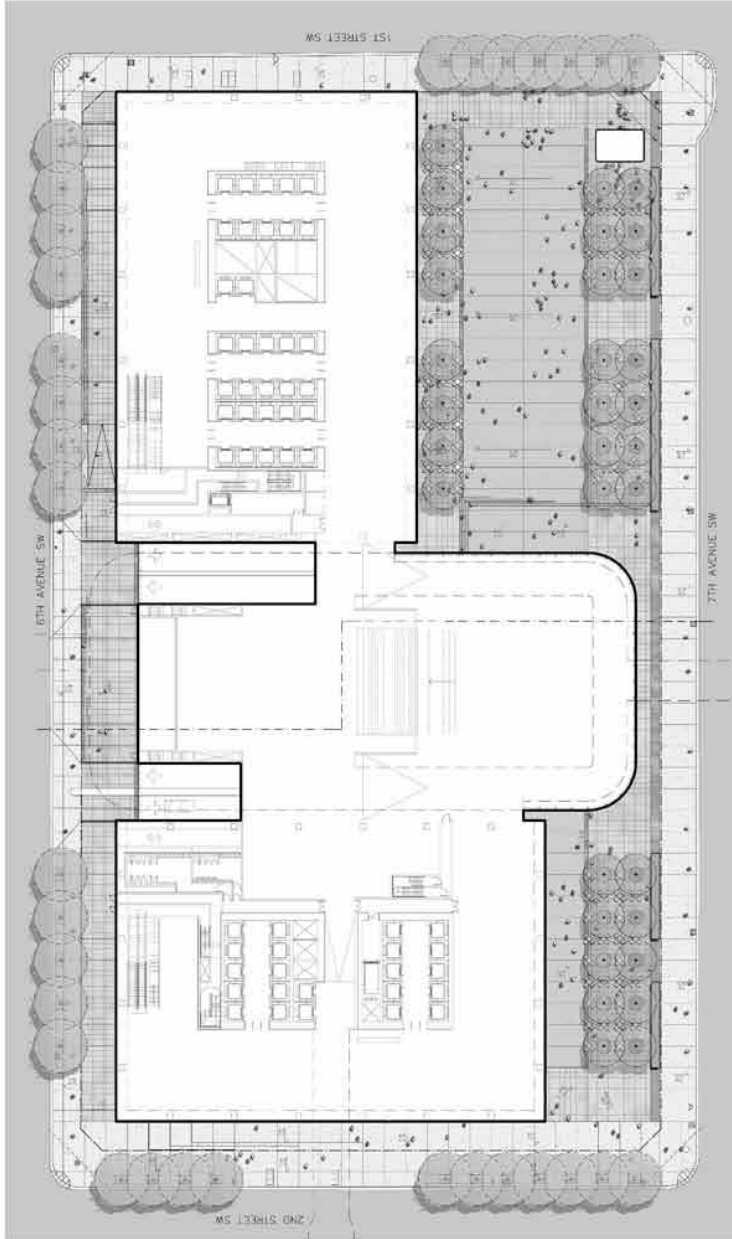
Brookfield

DIALOG
3215 WARD AVENUE SE
SUITE 100
DALLAS, TX 75226
TEL: 972.263.1051
FAX: 972.263.1051

WOODS BAGOT
78 OXFORD STREET
SUITE 100
UNIVERSITY MICROFILMS
ANN ARBOR, MI 48106
TEL: 734.763.6600
F: 734.763.6600

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DIALOG PROJECT NO. 08963208



1 SITE & LANDSCAPE ILLUSTRATIVE PLAN

GENERAL NOTES:
1. THE DRAWING HAS BEEN PREPARED FOR LANDSCAPE CONSULTANT PURPOSES. IT IS NOT TO BE USED FOR CONSTRUCTION PURPOSES WITHOUT THE LANDSCAPE ARCHITECT'S PERMISSION.
2. THE DRAWING IS TO BE USED TO OBTAIN PERMITS AND TO OBTAIN INFORMATION REGARDING THE PLANTING OF TREES AND LANDSCAPE MATERIALS.
3. ALL TREE PLANTING AREAS TO BE INDICATED WITH A TREE SYMBOL. TREE SYMBOLS WILL BE INDICATED WITH THE TREE SYMBOL AND TREE SPECIES.
4. STREET TREE AREAS TO BE INDICATED WITH THE TREE SYMBOL.

PAVING SCHEDULE:

- LIGHT COLOURED GRANITE
- CLAY PAVING
- PERVIOUS COLOURED GRANITE
- PERVIOUS GRANITE
- PERVIOUS/PERVIOUS COLOURED GRANITE
- PERVIOUS GRANITE
- MIX-RANGE, EARTH TONE
- PERVIOUS GRANITE

SITE AMENITY/FURNITURE SCHEDULE:

- SEAT WALLS
- TREES (TR 1: STREET TREE)
- TREES (TR 2: PLAZA TREE)
- TREES (TR 3: CITY TREEMAN)
- TREES (TR 4: CITY TREEMAN)
- TREES (TR 5: CITY TREEMAN)
- TREES (TR 6: CITY TREEMAN)
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PLANTING SCHEDULE:

- TREE TYPE 1: STREET TREE
- TREE TYPE 2: PLAZA TREE
- TREE TYPE 3: CITY TREEMAN
- TREE TYPE 4: CITY TREEMAN
- TREE TYPE 5: CITY TREEMAN
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- TREE TYPE 39: CITY TREEMAN
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- TREE TYPE 41: CITY TREEMAN
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- TREE TYPE 77: CITY TREEMAN
- TREE TYPE 78: CITY TREEMAN
- TREE TYPE 79: CITY TREEMAN
- TREE TYPE 80: CITY TREEMAN



NEW SHEET

WOODS BAGOT DIALOG
225 SIXTH

LEGAL DESCRIPTION
LOT 10, DISTRICT 11
BLOCK 14,
PLAZA

DESIGN FIRM
— Development Project
— 076 11, Regent
2012.03.26

DRAWING TITLE
**MEZZANINE LEVEL
PLAN**

SCALE 1:250

GENERAL NOTES

- HATCH PATTERN, SETBACK &
CORNER CUTS INDICATED
DIMENSIONED



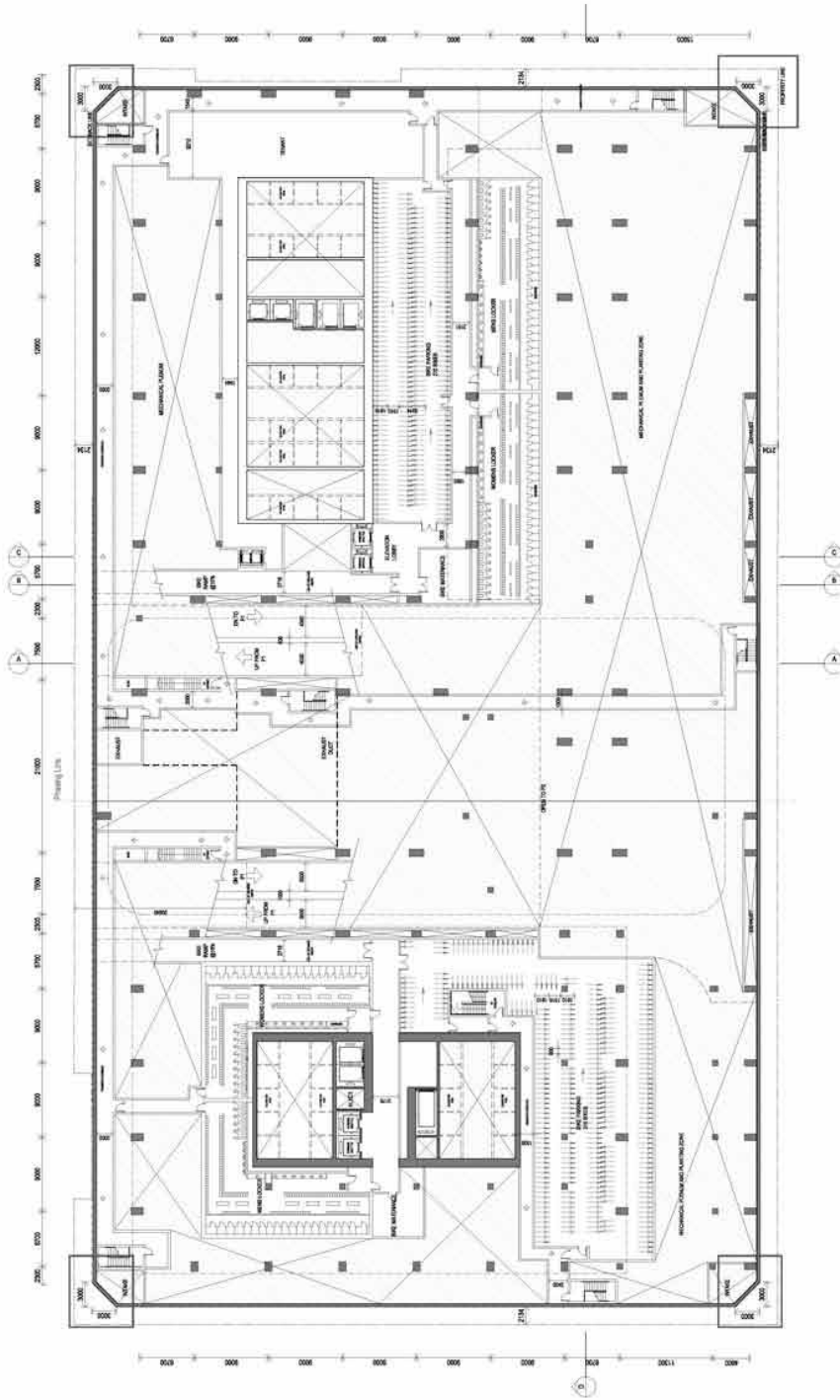
Brookfield

DIALOG 11 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.243.4521
F: 403.228.2024

WOODS BAGOT
76 AVONROD STREET
VANCOUVER, BC
V6V 1S5
UNITED KINGDOM
TEL: +44 (0) 20 7337 8200
F: +44 (0) 20 7337 8200

38

DIALOG PROJECT NO: 0396C-2006



BIKE FACILITY SUMMARY

	PHASE 1	PHASE 2	TOTAL
BIKES	270	372	642
LOCKERS	22	34	56
BIKE REPAIR	1	1	2



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 10, DISTRICT 11
BLOCK 14,
PLATA

PROJECT FIDR
- Development Permit
- OTR 1 - Rezone
2012.11.29
2012.03.09

DRAWING TITLE
**LEVEL 1 PARKING
PLAN**

SCALE: 1:250

GENERAL NOTES:
- All parking spaces have a minimum width of 2.600m.

- PROPERTY LINE - SETBACK &
CORNER CURBS HIGHLIGHTED
- DIMENSIONED
- AMERICAN PARALLEL SUMMARY
- WATER METER ROOM
- RELOCKED



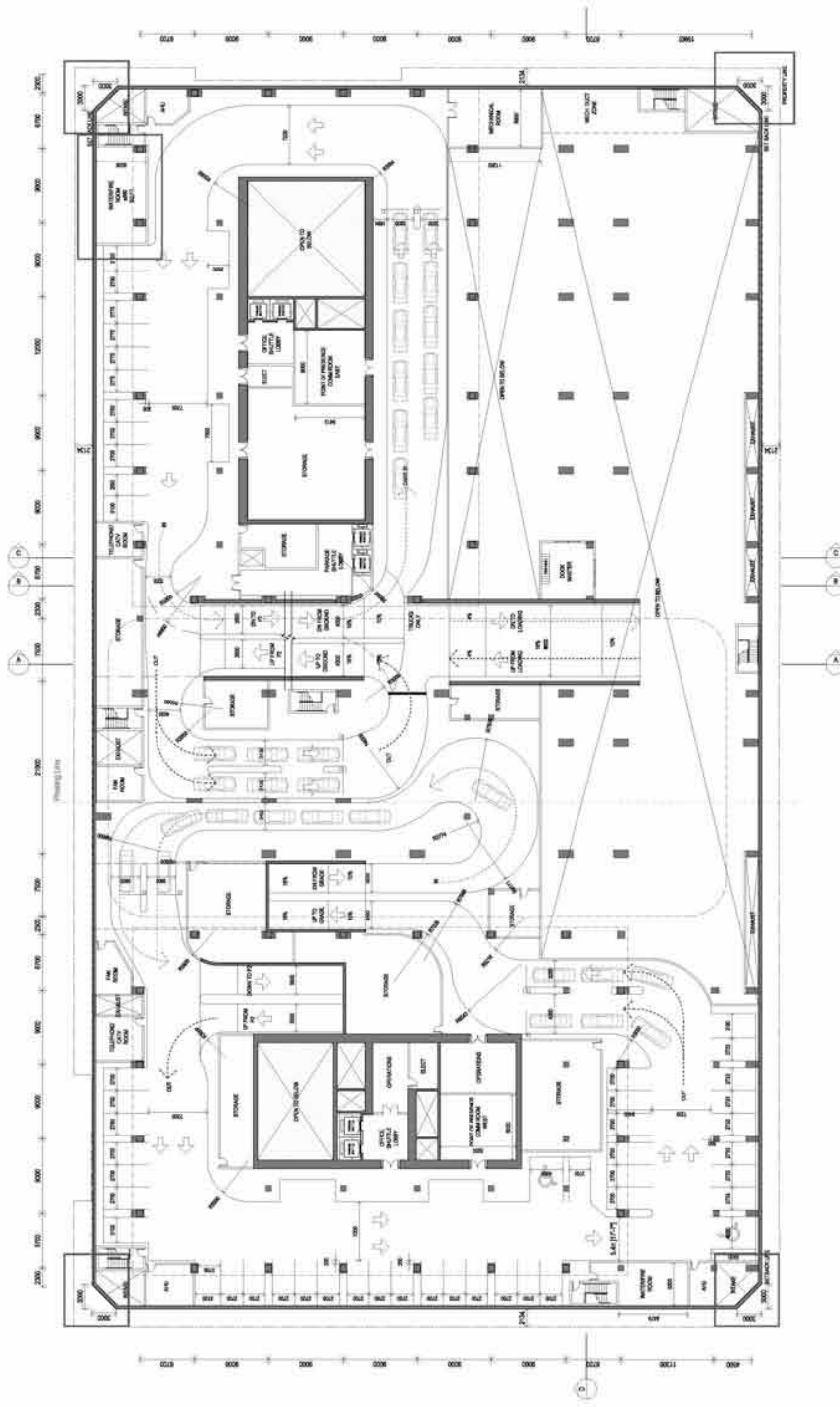
Brookfield

DIALOG
111 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.243.4551
F: 403.229.0544

WOODS BAGOT
76 AVONROD STREET
UNIT 103S
UNIT 103S
UNIT 103S
UNIT 103S
F: 44.20.7037.8000

39

DIALOG PROJECT NO: 09866208



PARKING SUMMARY 4-

PHASE	1	2	3	4	5
P1	34				
P2	36				
P3	38				
P4	24				
P5	24				
P6	24				
P7	24				
P8	24				
P9	24				
P10	24				
P11	24				
P12	24				
P13	24				
P14	24				
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P96	24				
P97	24				
P98	24				
P99	24				
P100	24				
TOTAL	2400				

**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 150, DISTRICT
BLOCK 14,
PLANNED AREA

SUBJECT FOUR
Development Project
- 070 - 1 - Regeneration
2012.11.25
2012.03.28

DRAWING TITLE
**LEVEL 2 PARKING
PLAN**

SCALE
1:250

GENERAL NOTES
All parking spaces have a minimum width of 2.600m.

PROPERTY LINE - SETBACK &
CORNER CUTS HIGHLIGHTED
DASHED
ADJACENT PARCEL SUMMARY



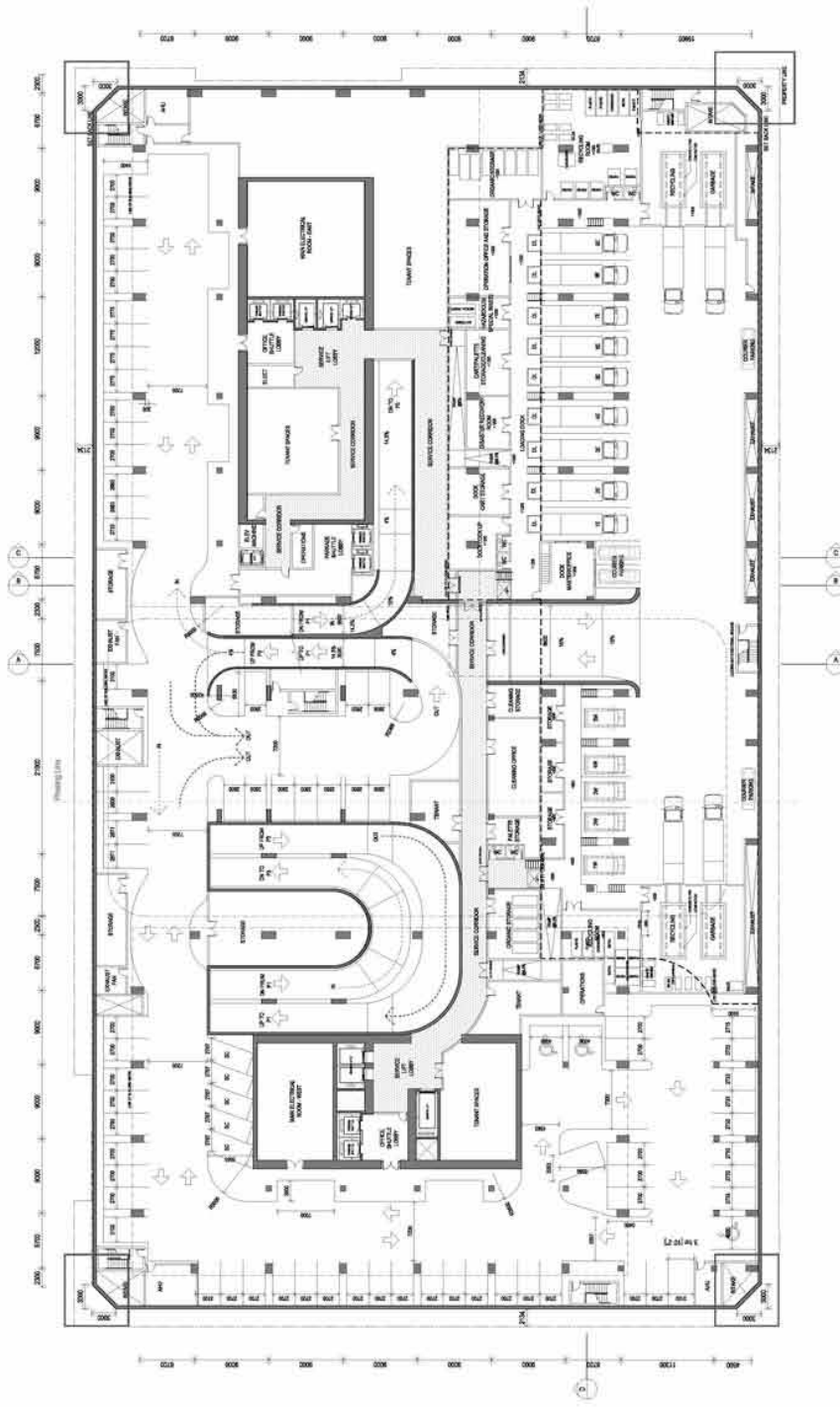
Brookfield

DIALOG 111 AVENUE SE
CALGARY, AB
T2G 0G5
F: 403.239.3044

WOODS BAGOT
75 OXFORD STREET
UNIT 1005
UNIT 1005
F: 403.703.7030

40

DIALOG PROJECT NO: 0996C1306



PARKING SUMMARY 4-

PHASE	
P1	34
P2	95
P3	224
P4	234
P5	234
P6	234
P7	234
P8	234
TOTAL LEVEL 2	
LOADING BAY	1
WAITING	4
WALKWAY	1
WHEELCHAIR	1
CORPORATE	3
TOTAL	1080

WOODS BAGOT DIALOG
225 SIXTH

LEGAL DESCRIPTION
 LOT 10, DISTRICT
 BLOCK 14,
 PLAINA

REVISION
 2012.11.29
 2012.03.09

DRAWING TITLE
**LEVEL 7 PARKING
 PLAN**

SCALE: 1/2"=1'-0"
 GENERAL NOTES:
 All parking spaces have a minimum width of 20.00m.
 - PROPERTY LINE, SETBACK &
 CORNER CUTS INDICATED
 UNDESIGNSHED
 - ADDED PERMISSIVE SUMMARY



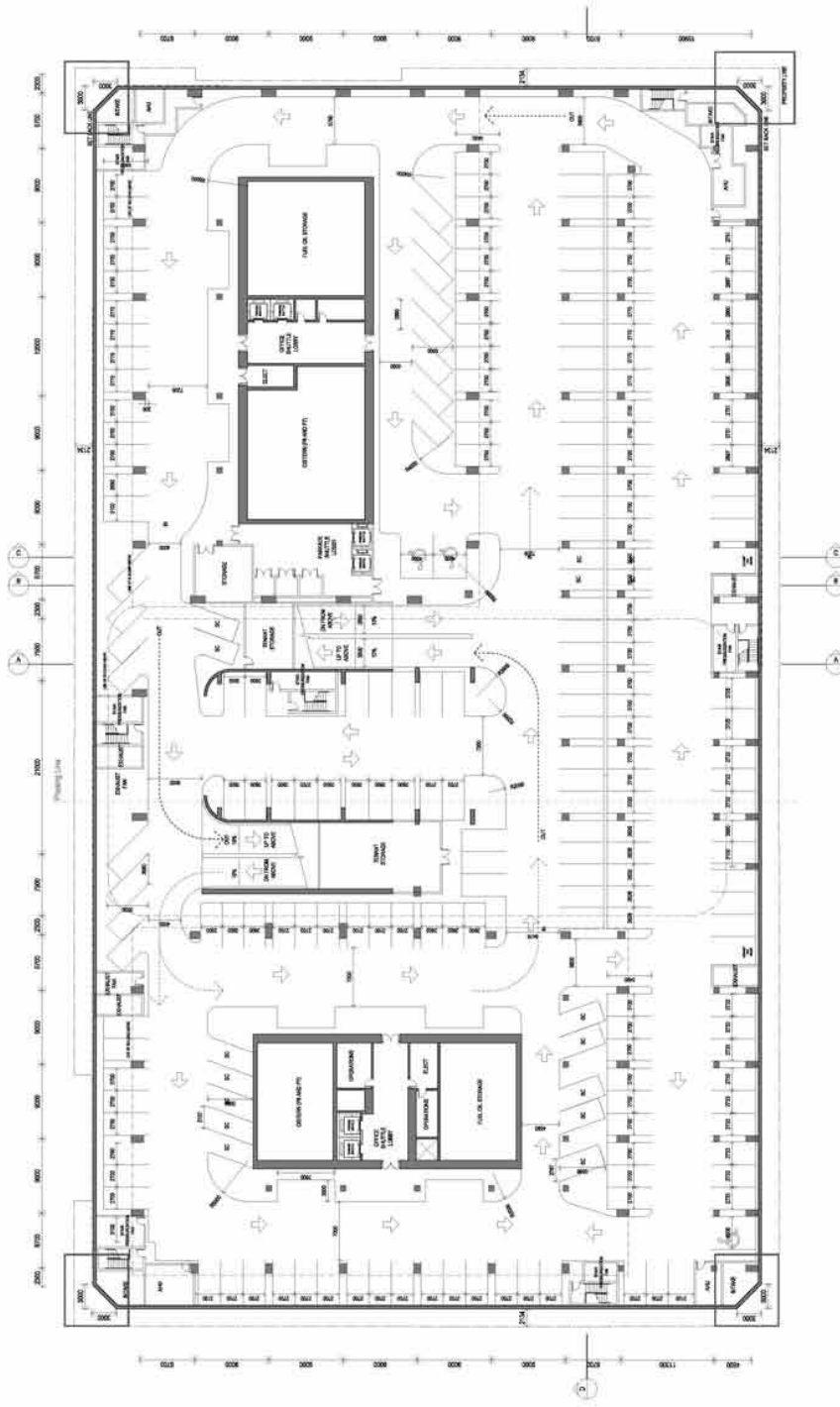
Brookfield

DIALOG
 100 W. AVENUE 5E
 OAK GARY, AB
 T2E 0S5
 TEL: 416 291 4551
 F: 416 291 5051

WOODS BAGOT
 76 WYFORD STREET
 WINDY HILL, ONTARIO
 L0R 2S5
 UNITED KINGDOM
 TEL: +44 (0)1235 433920
 FAX: +44 (0)1235 433922

43

DIALOG PROJECT NO. 03963208



PARKING SUMMARY 4-

NUMBER	DESCRIPTION	AREA (SQM)
P1	PA	34
P2	PB	85
P3	PC	204
P4	PD	224
P5	PE	224
P6	PF	224
P7	PG	224
P8	PH	224
P9	PI	224
P10	PJ	224
P11	PK	224
P12	PL	224
P13	PM	224
P14	PN	224
P15	PO	224
P16	PP	224
P17	PQ	224
P18	PR	224
P19	PS	224
P20	PT	224
P21	PV	224
P22	PW	224
P23	PX	224
P24	PY	224
P25	PZ	224
P26	QA	224
P27	QB	224
P28	QC	224
P29	QD	224
P30	QE	224
P31	QF	224
P32	QG	224
P33	QH	224
P34	QI	224
P35	QJ	224
P36	QK	224
P37	QL	224
P38	QM	224
P39	QN	224
P40	QO	224
P41	QP	224
P42	QQ	224
P43	QR	224
P44	QS	224
P45	QT	224
P46	QU	224
P47	QV	224
P48	QW	224
P49	QX	224
P50	QY	224
P51	QZ	224
P52	RA	224
P53	RB	224
P54	RC	224
P55	RD	224
P56	RE	224
P57	RF	224
P58	RG	224
P59	RH	224
P60	RI	224
P61	RJ	224
P62	RK	224
P63	RL	224
P64	RM	224
P65	RN	224
P66	RO	224
P67	RP	224
P68	RQ	224
P69	RS	224
P70	RT	224
P71	RU	224
P72	RV	224
P73	RW	224
P74	RX	224
P75	RY	224
P76	RZ	224
P77	SA	224
P78	SB	224
P79	SC	224
P80	SD	224
P81	SE	224
P82	SF	224
P83	SG	224
P84	SH	224
P85	SI	224
P86	SJ	224
P87	SK	224
P88	SL	224
P89	SM	224
P90	SN	224
P91	SO	224
P92	SP	224
P93	SQ	224
P94	SR	224
P95	SS	224
P96	ST	224
P97	SU	224
P98	SV	224
P99	SW	224
P100	SX	224
P101	SY	224
P102	SZ	224
P103	TA	224
P104	TB	224
P105	TC	224
P106	TD	224
P107	TE	224
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P300	TF	224

**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 14,
PLATA

DESIGN FIRM
Development Project
- 07/11/13
2012.03.06

DRAWING TITLE
MAIN LEVEL PLAN

SCALE
1:250

GENERAL NOTES

- AMBITIOUS DRIVEWAY FLAPES
- DIMENSIONED DRIVEWAY
- THROAT & FLAPES
- PROPERTY LINE, SETBACK & CORNER CUTS INDICATED
- DIMENSIONED



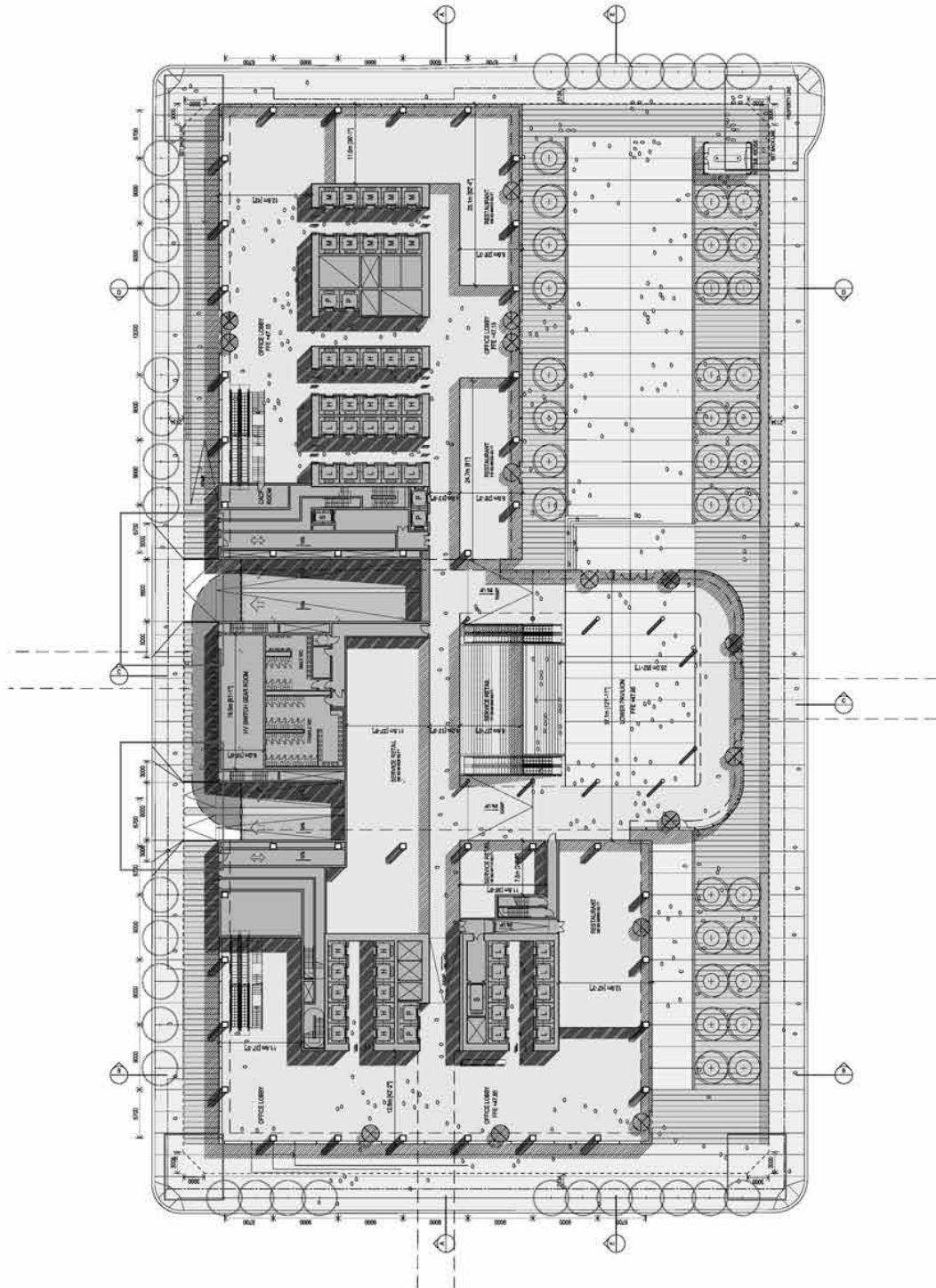
Brookfield

DIALOG
11 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.278.1551
F: 403.278.1551

WOODS BAGOT
76 OXFORD STREET
UNIT 105
UNITED KINGDOM
W1D 1SS
TEL: +44 (0) 20 7337 6000
F: +44 (0) 20 7337 6000

44

DIALOG PROJECT NO: 098961206



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 10, DIST. 11
BLOCK 14,
PLATA

DESIGN DATE
2012.11.29
2012.03.06

DRAWING TITLE
+15 LEVEL PLAN

SCALE
1:250

GENERAL NOTES

- 1. Floor Slab
- 2. Floor Finish
- 3. All Footing
- 4. (See Section)
- 5. Service Location

Refer To: Refer to Exchange Plan

- PROPERTY LINE SETBACK &
DIMENSIONS INDICATED



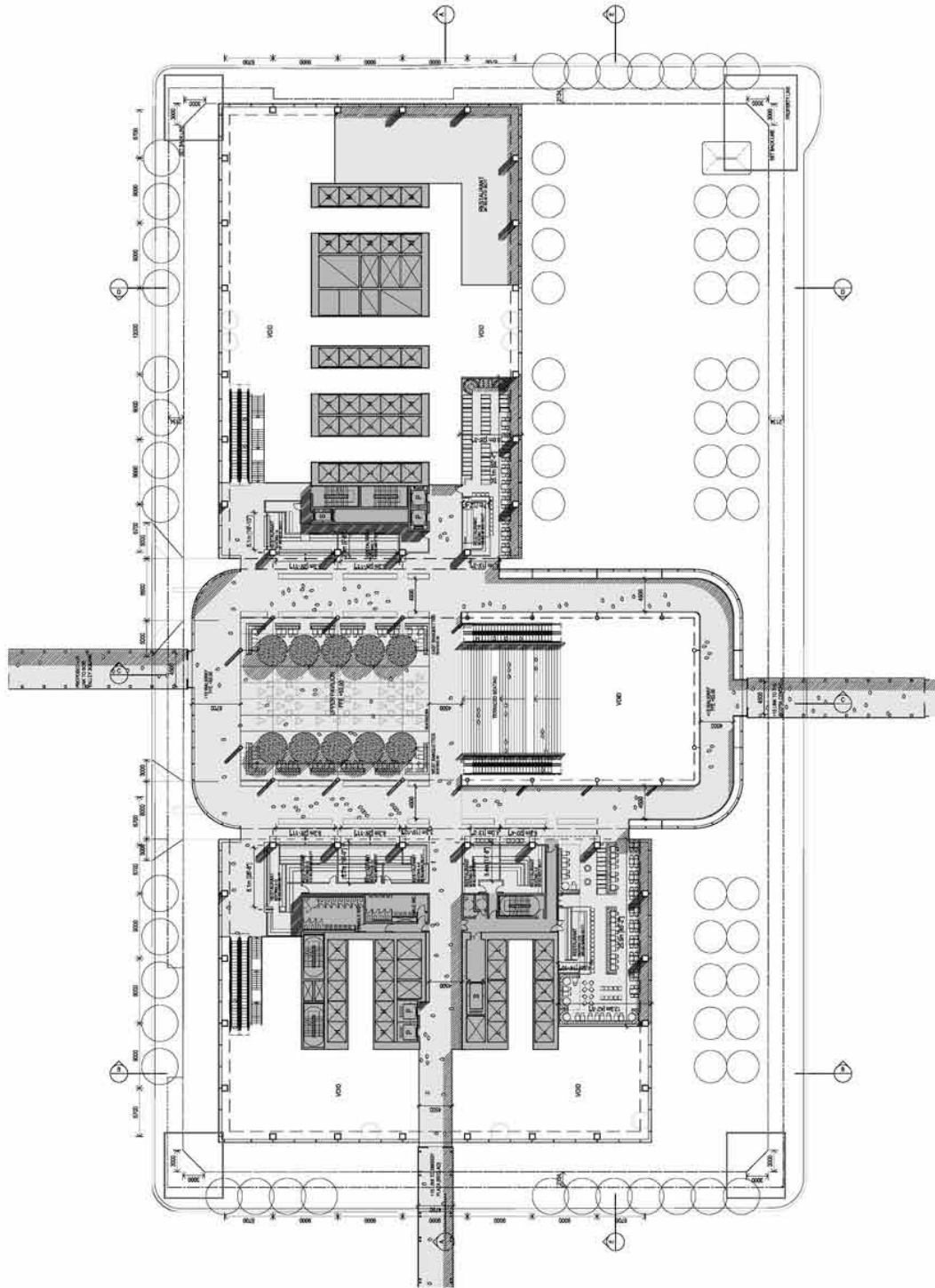
Brookfield

DIALOG
11 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.278.4551
F: 403.278.0541

WOODS BAGOT
76 AVONDALE STREET
UNIT 105
UNITED KINGDOM
F: +44 20 7337 6000

45

DIALOG PROJECT NO: 0986C-106



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 44
PLANA

DESIGNER
Development Project
- DTB 1 - Regeneration
2012.11.29
2013.03.06

**DRAWING TITLE
LOW RISE TOWER
FLOOR PLAN**

SCALE
1:250

GENERAL NOTES

CODE SYNOPSIS

- 1. 15000 PROPOSED
- 2. WORKING PLAN BY TRENDS (STORAGE)
- 3. WORKING PLAN BY TRENDS (OFFICE)
- 4. WORKING PLAN BY TRENDS (CORRIDOR)
- 5. WORKING PLAN BY TRENDS (LIFT)
- 6. WORKING PLAN BY TRENDS (MECHANICAL)
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- 99. WORKING PLAN BY TRENDS (MECHANICAL)
- 100. WORKING PLAN BY TRENDS (MECHANICAL)

MEASUREMENTS

NOTES

REVISIONS

DATE

BY

DESCRIPTION

APPROVED

DATE

BY

DESCRIPTION

APPROVED

DATE

BY

DESCRIPTION

APPROVED

DATE

BY

DESCRIPTION

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DESCRIPTION

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BY

DESCRIPTION



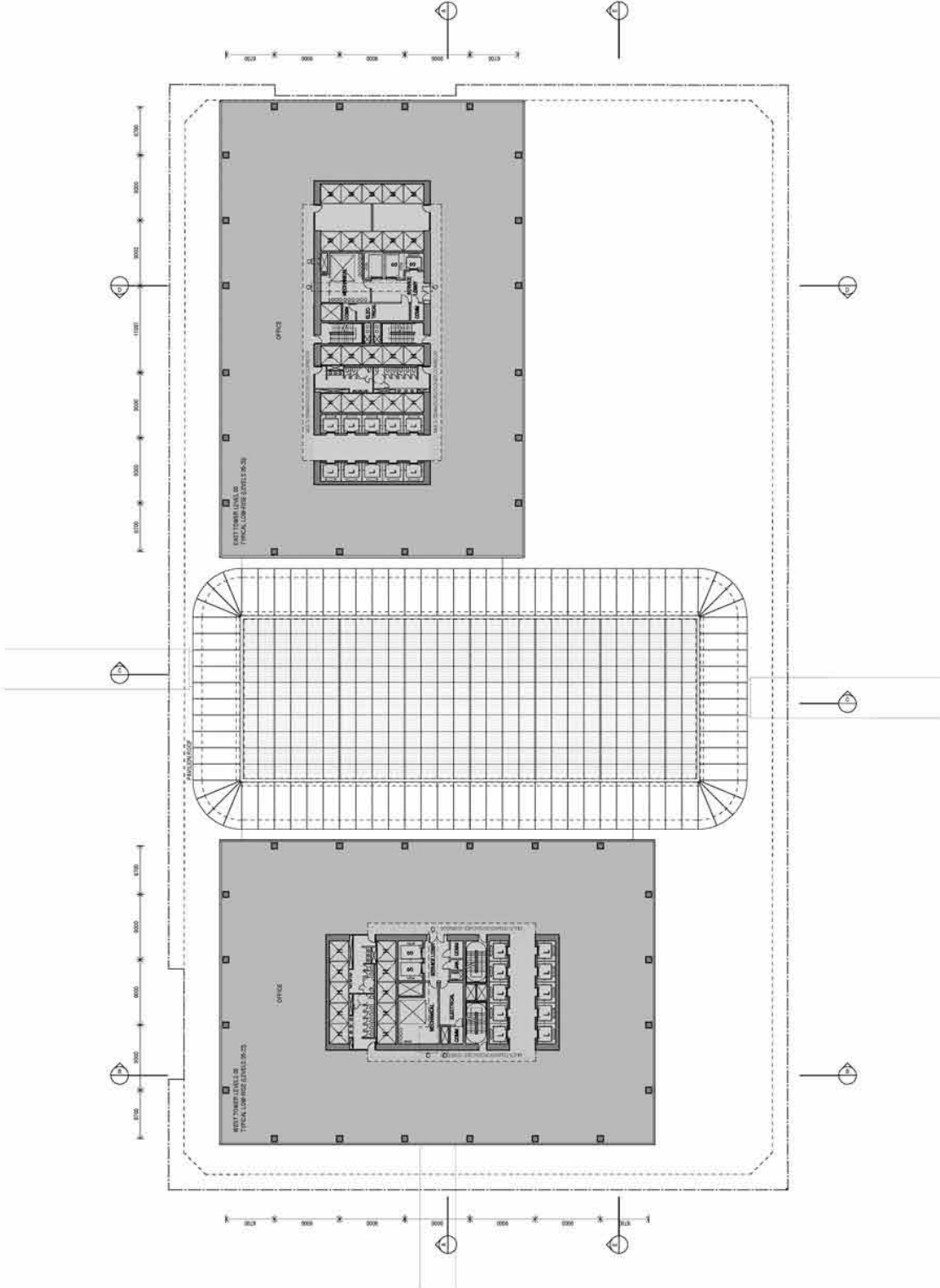
Brookfield

DIALOG
100 KING AVENUE SE
CALGARY, AB
T2G 0G5
TEL: 403.228.4521
F: 403.228.0504

WOODS BAGOT
76 WYNDHAM STREET
WINDSOR, ON
N9A 1G5
UNITED KINGDOM
TEL: +44 (0) 1537 40000
F: +44 (0) 1537 40000

47

DIALOG PROJECT NO: 0986C-006



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 14,
PLAZA

ISSUED FOR
Development Permit
- DTB's Requirements
2012.11.29
2012.03.06

DRAWING TITLE
ROOF PLAN

1:250

SCALE

GENERAL NOTES



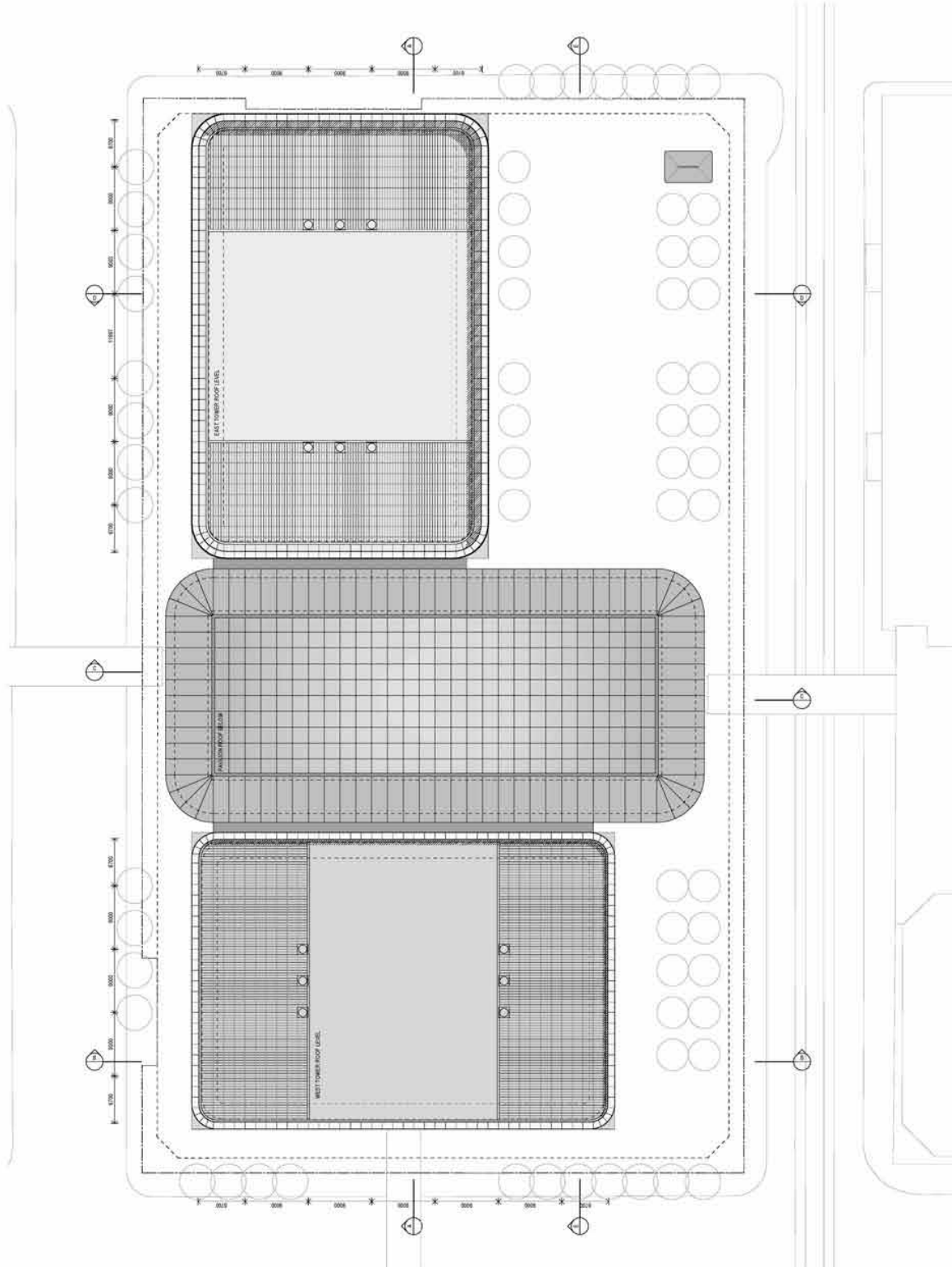
Brookfield

DIALOG
225 SIXTH AVENUE SE
CALGARY, AB
T2G 0G5
TEL: 403.278.4551
F: 403.278.7054

WOODS BAGOT
76 WYNDHAM STREET
WINDSOR, ON
M90 1G5
UNITED KINGDOM
TEL: +44 (0) 1537 43330
F: +44 (0) 1537 43330

54

DIALOG PROJECT NO: 0986C-208



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 44
PLATA

DESIGN FOUR
Development Project
- 078-11-Regenerative
2012.11.25
2012.03.06

DRAWING TITLE
SOUTH ELEVATION

SCALE: 1/8"=1'-0"

GENERAL NOTES

- 1. SEE ARCHITECTURAL PLAN FOR
- 2. SEE ARCHITECTURAL PLAN FOR
- 3. STRUCTURAL COLUMN PLACING
- 4. METAL CLADDING
- 5. SURFACE FINISHES
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- 7. METAL CLADDING
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PROPERTY & SETBACK LINES
INDICATED



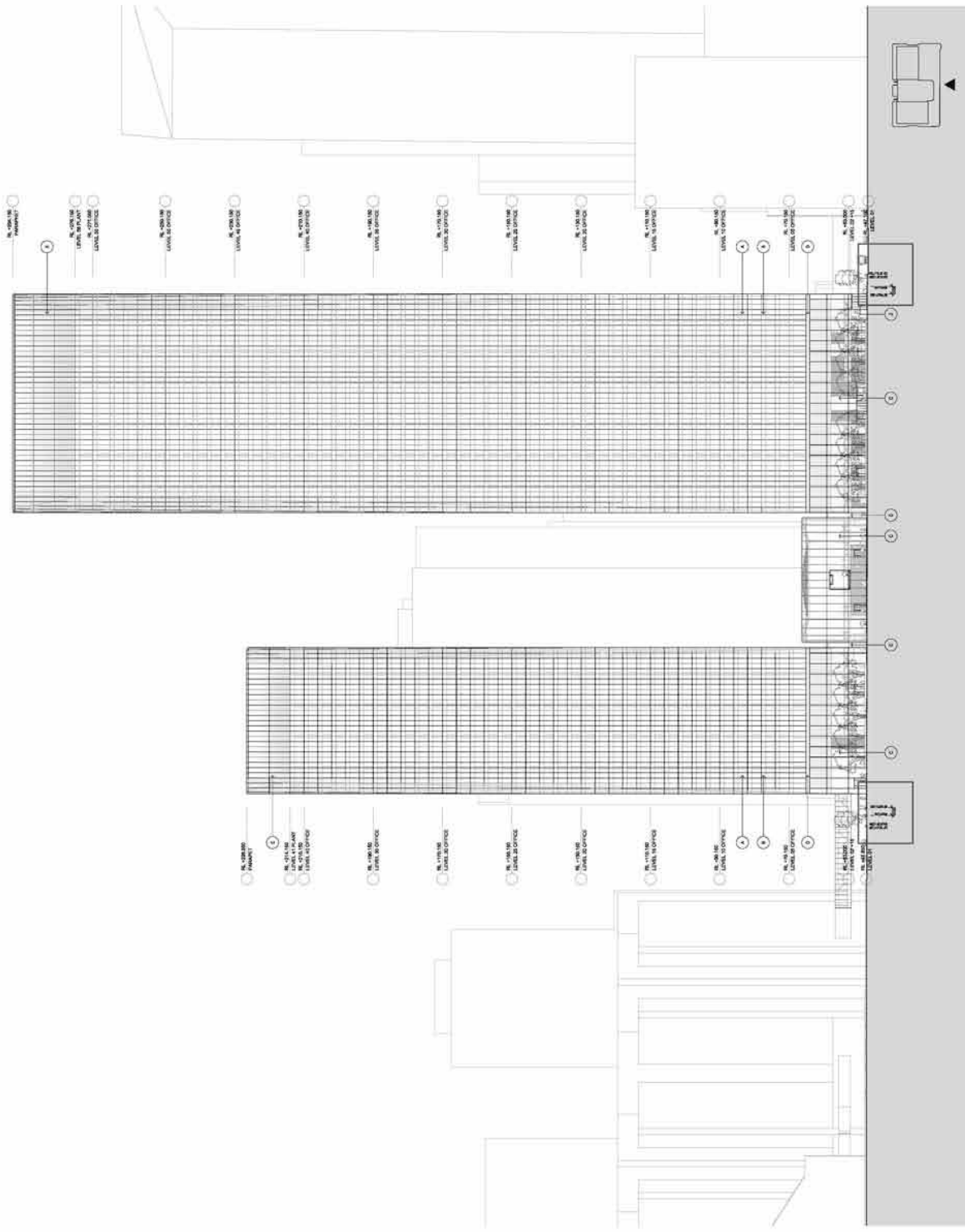
Brookfield

DIALOG
111 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.243.4551
F: 403.228.0504

WOODS BAGOT
76 AVONDALE STREET
VANCOUVER, BC
V6J 1G5
TEL: 604.681.2222
F: 604.271.3337 (toll free)

55

DIALOG PROJECT NO. 038662208



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 48,
PLATA

DESIGN FOUR
Developmental Project
- 07/11/13, 13/13/13
2012.03.06

DRAWING TITLE
EAST ELEVATION

SCALE: 1:500

GENERAL NOTES

- A. SEE SECTION PANELS
- B. SEE DIMENSIONS PANEL WITH THIS DRAWING
- C. STRUCTURAL CLEAR CLADDING
- D. METAL CLADDING
- E. SURFACE FINISHES
- F. SURFACE FINISHES
- G. SURFACE FINISHES
- H. METAL CLADDING
- I. METAL CLADDING
- J. SEE CLADDING TO BOOK

PROPERTY & SETBACK LINES INDICATED



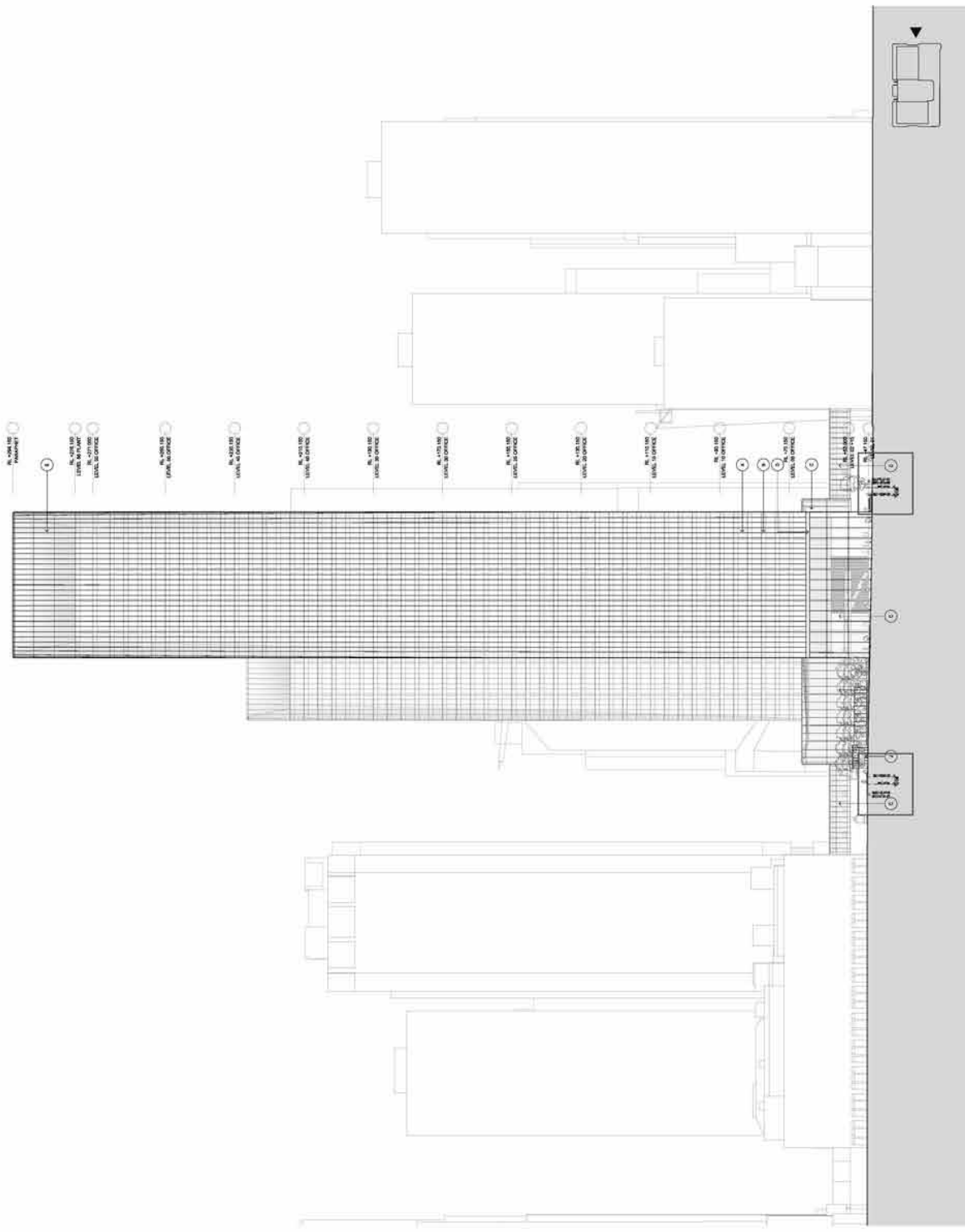
Brookfield

DIALOG
111 AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.228.4521
F: 403.228.7024

WOODS BAGOT
76 DUNDAS STREET
UNIT 1005
TORONTO, ONT.
M5G 1K5
TEL: 416.763.8880
F: 416.763.7000

56

DIALOG PROJECT NO. 03986C-008



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 14,
PLATA

SHEET FOUR
Development Permit
- 076-1-Reg-0000
2012.11.29
2012.03.06

DRAWING TITLE
NORTH ELEVATION

SCALE
1:500

GENERAL NOTES

- 1. SEE GENERAL NOTES SHEET 01
- 2. SEE GENERAL NOTES SHEET 02
- 3. SEE GENERAL NOTES SHEET 03
- 4. SEE GENERAL NOTES SHEET 04
- 5. SEE GENERAL NOTES SHEET 05
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- 100. SEE GENERAL NOTES SHEET 100

PROPERTY & DETACHMENT LINES
INDICATED
- LEGEND APPLICABLE



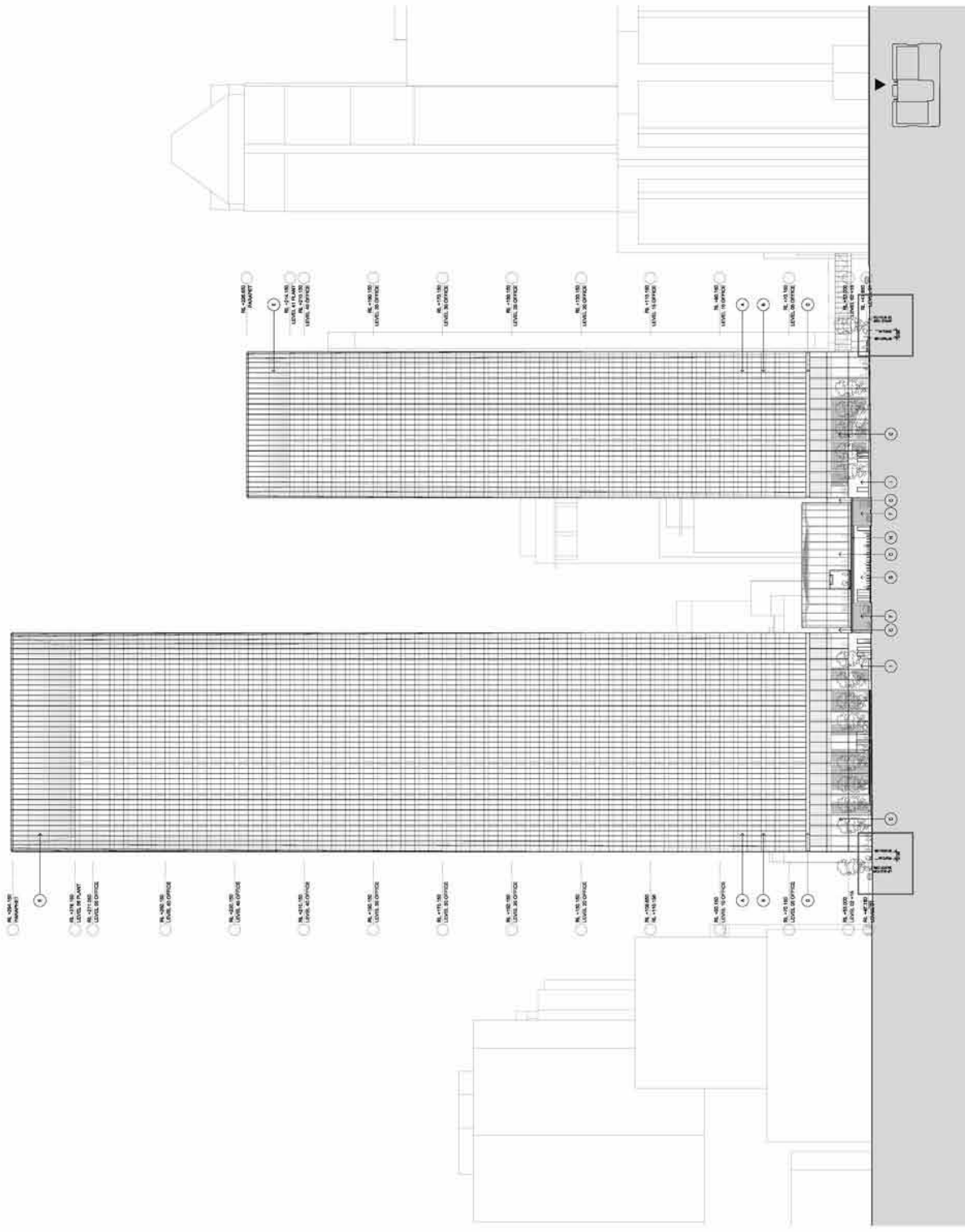
Brookfield

DIALOG
1000 AVENUE SE
CALGARY, AB
T2G 0G5
TEL: 403.243.1451
F: 403.228.0504

WOODS BAGOT
76 AVONDALE STREET
VANCOUVER, BC
V6P 1G5
TEL: 604.273.1000
F: 604.273.1000

57

DIALOG PROJECT NO: 038662206



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 14,
PLANNED

DESIGNED FOR
Developmental Project
- DTB's 11th Regeneration
REVISED 25
2012 03 28

DRAWING TITLE
WEST ELEVATION

SCALE: 1:500

GENERAL NOTES

- 1. SEE ARCHITECT'S GENERAL NOTES
- 2. SEE ARCHITECT'S GENERAL NOTES
- 3. STRUCTURAL CLASH DRAWING
- 4. METAL CLADDING
- 5. SURFACE FINISHES
- 6. VOLUME RENDERING
- 7. METAL CLADDING
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PROPERTY & SETBACK LINES
INDICATED



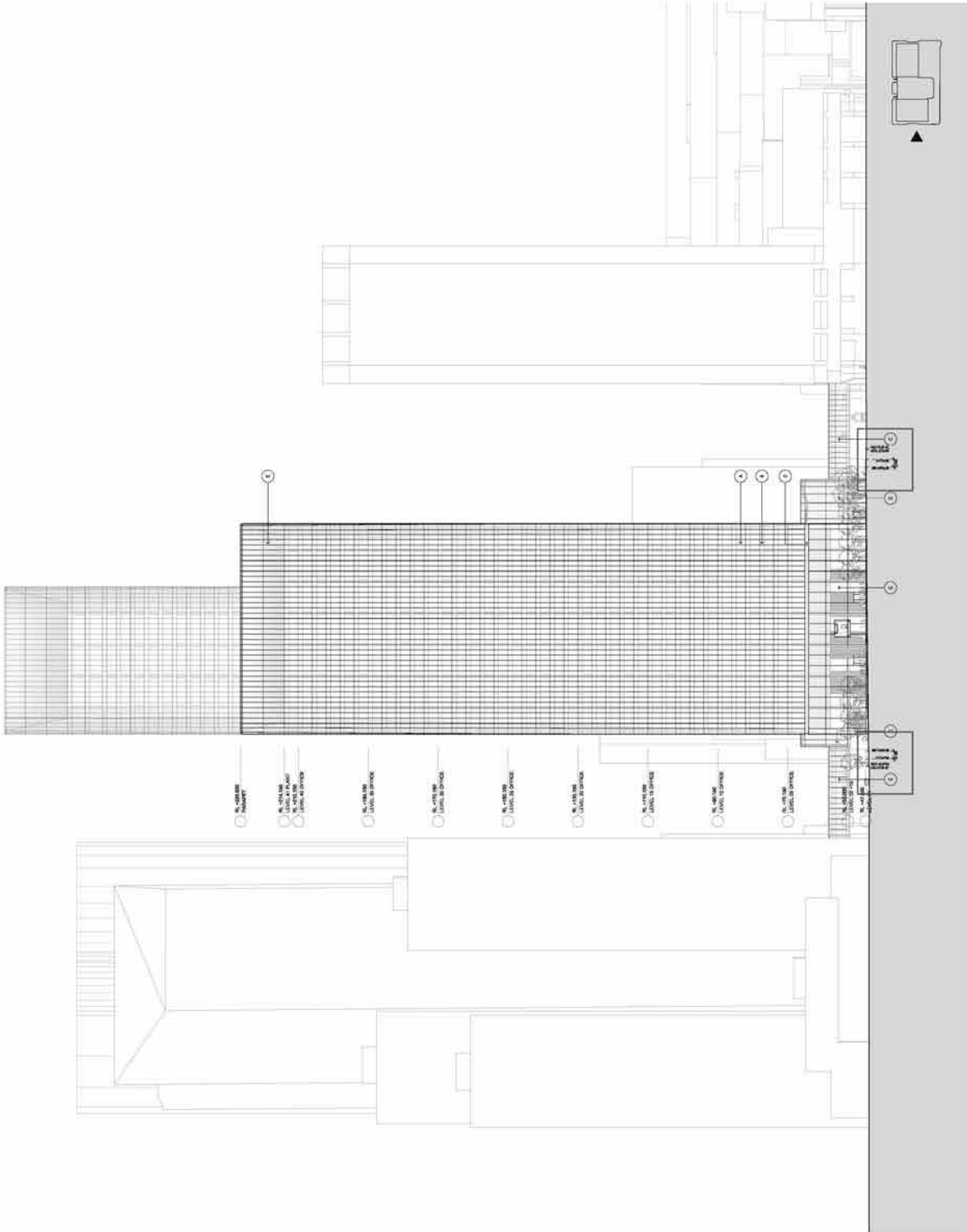
Brookfield

DIALOG
111 AVENUE SE
CALGARY, AB
T2C 0S5
TEL: (403) 228-1000
F: (403) 228-1004

WOODS BAGOT
76 AVONDALE STREET
VANCOUVER, BC
V6P 1G5
TEL: (604) 681-3000
F: (604) 681-3000

58

DIALOG PROJECT NO: 099602208



**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 14,
PLANA

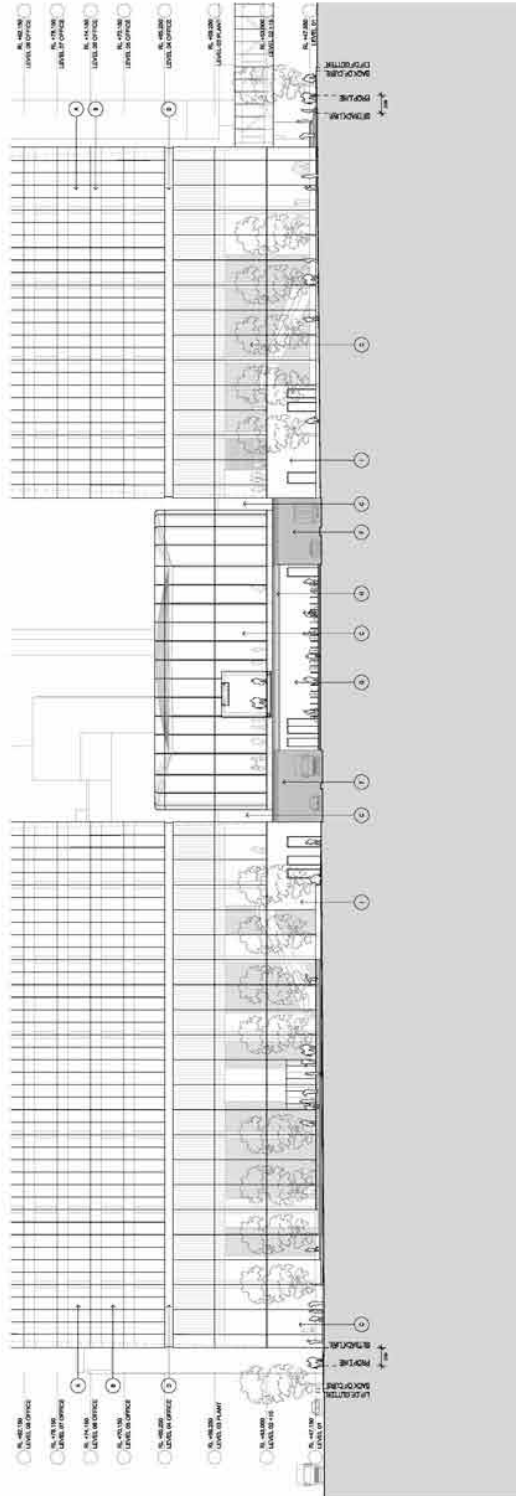
DESIGN FOUR
Development Project
- 078-11-Regeneration
2012.11.25
2012.03.06

**DRAWING TITLE
SOUTH STREETSCAPE
ELEVATION**

SCALE: 1:250

GENERAL NOTES

- 1. SEE ARCHITECT'S GENERAL NOTES
- 2. SEE ARCHITECT'S GENERAL NOTES
- 3. STRUCTURAL CLEAR HEIGHT
- 4. METAL CLADDING
- 5. SURFACE FINISH
- 6. SQUARE BUTTER
- 7. METAL LOUVER
- 8. SEE CLADDING TO BOOK



Brookfield

DIALOG
225 SIXTH AVENUE SE
CALGARY, AB
T2E 0S5
TEL: 403.278.4551
F: 403.278.0504

WOODS BAGOT
76 WYNDHAM STREET
UNIVERSITY CITY
UNIVERSITY CITY
UNIVERSITY CITY
F: 403.278.0504

59

DIALOG PROJECT NO: 09886208

**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 44,
PLAN 1

DESIGN FIRM
— Development Project
— 078-11-Regeneration
2012.11.25
2012.03.06

**DRAWING TITLE
EAST STREETSCAPE
ELEVATION**

SCALE
1:250

GENERAL NOTES

- A. SEE SECTION PANEL
- B. SEE DIMENSIONS PANEL WITH
- C. SEE DIMENSIONS PANEL WITH
- D. STRUCTURAL CLASH DRAWING
- E. MEAN DIMENSIONS
- F. SURFACE FINISHES
- G. VOLUME DOTTING
- H. MEAN LOUVER
- I. SEE DIMENSIONS TO BOOK



Brookfield

DIALOG
225 SIXTH AVENUE SE
CALGARY, AB
T2C 0G5
TEL: 403.278.4521
F: 403.278.3524

WOODS BAGOT
76 WYNDHAM STREET
UNIT 1005
UNITED KINGDOM
W1A 0AA
TEL: +44 (0) 20 7537 4000
F: +44 (0) 20 7537 4000

60

DIALOG PROJECT NO: 038562-006

**WOODS BAGOT DIALOG
225 SIXTH**

LEGAL DESCRIPTION
LOT 1 TO 11
BLOCK 14,
PLATA

DESIGN FIRM
— Development Project
— 078-1, Regeneration
2012.11.29
2013.03.06

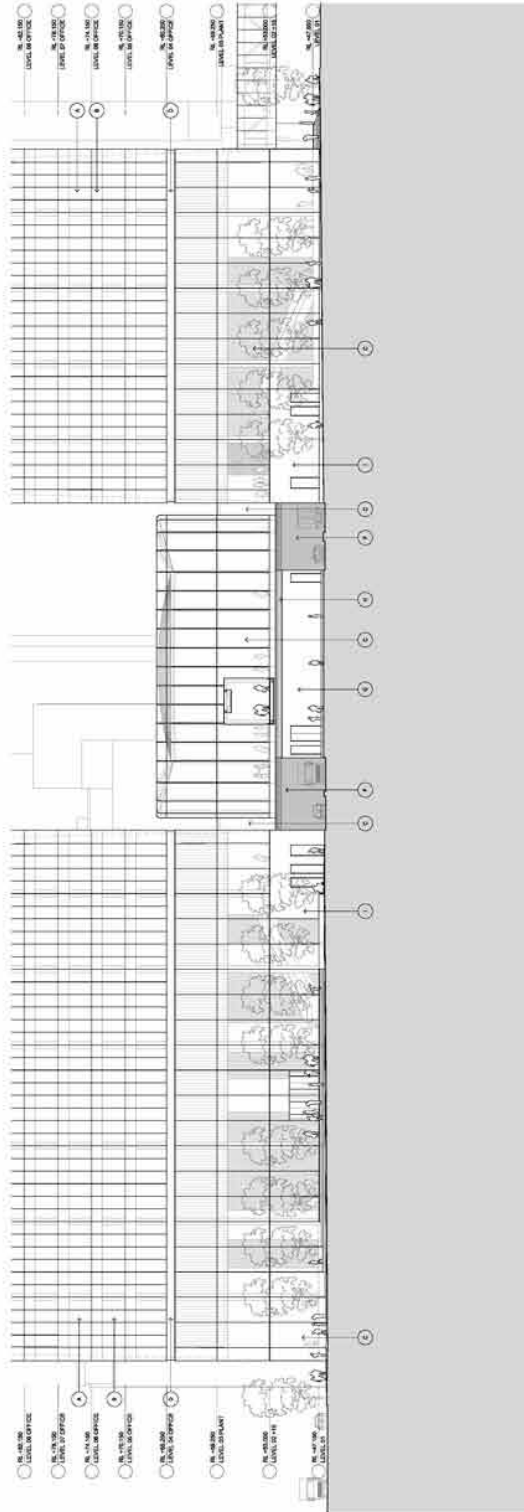
**DRAWING TITLE
NORTH STREETSCAPE
ELEVATION**

SCALE
1:250

GENERAL NOTES

- 1. SEE ARCHITECT'S GENERAL NOTES
- 2. PROVIDE METAL BACKING
- 3. PROVIDE METAL CLADDING
- 4. METAL CLADDING
- 5. SEE ARCHITECT'S GENERAL NOTES
- 6. SEE ARCHITECT'S GENERAL NOTES
- 7. SEE ARCHITECT'S GENERAL NOTES
- 8. SEE ARCHITECT'S GENERAL NOTES
- 9. SEE ARCHITECT'S GENERAL NOTES
- 10. SEE ARCHITECT'S GENERAL NOTES
- 11. SEE ARCHITECT'S GENERAL NOTES
- 12. SEE ARCHITECT'S GENERAL NOTES

LEGEND (REVISED)



Brookfield

DIALOG
225 SIXTH AVENUE SE
CALGARY, AB
T2E 0S5
TEL: 403.278.4551
F: 403.278.0504

WOODS BAGOT
76 AVONFORD STREET
UNIT 105
UNITED KINGDOM
F: +44 (0) 1937 40000

61

DIALOG PROJECT NO: 0896C-208

WOODS BAGOT DIALOG
225 SIXTH

LEGAL DESCRIPTION
BLOCK 414
PLATA

RENDER FOR:
Development Permit
2012.11.29
2012.12.28

DRAWING TITLE
SITE SECTION AA

SCALE
1:500

GENERAL NOTES

- A. SEE DRAWINGS FOR
- B. PROPOSED MATERIALS WITH
- C. FINISHES AND TOLERANCES
- D. FINISH SCHEDULE
- E. FINISH SCHEDULE
- F. FINISH SCHEDULE
- G. FINISH SCHEDULE
- H. FINISH SCHEDULE
- I. FINISH SCHEDULE
- J. FINISH SCHEDULE



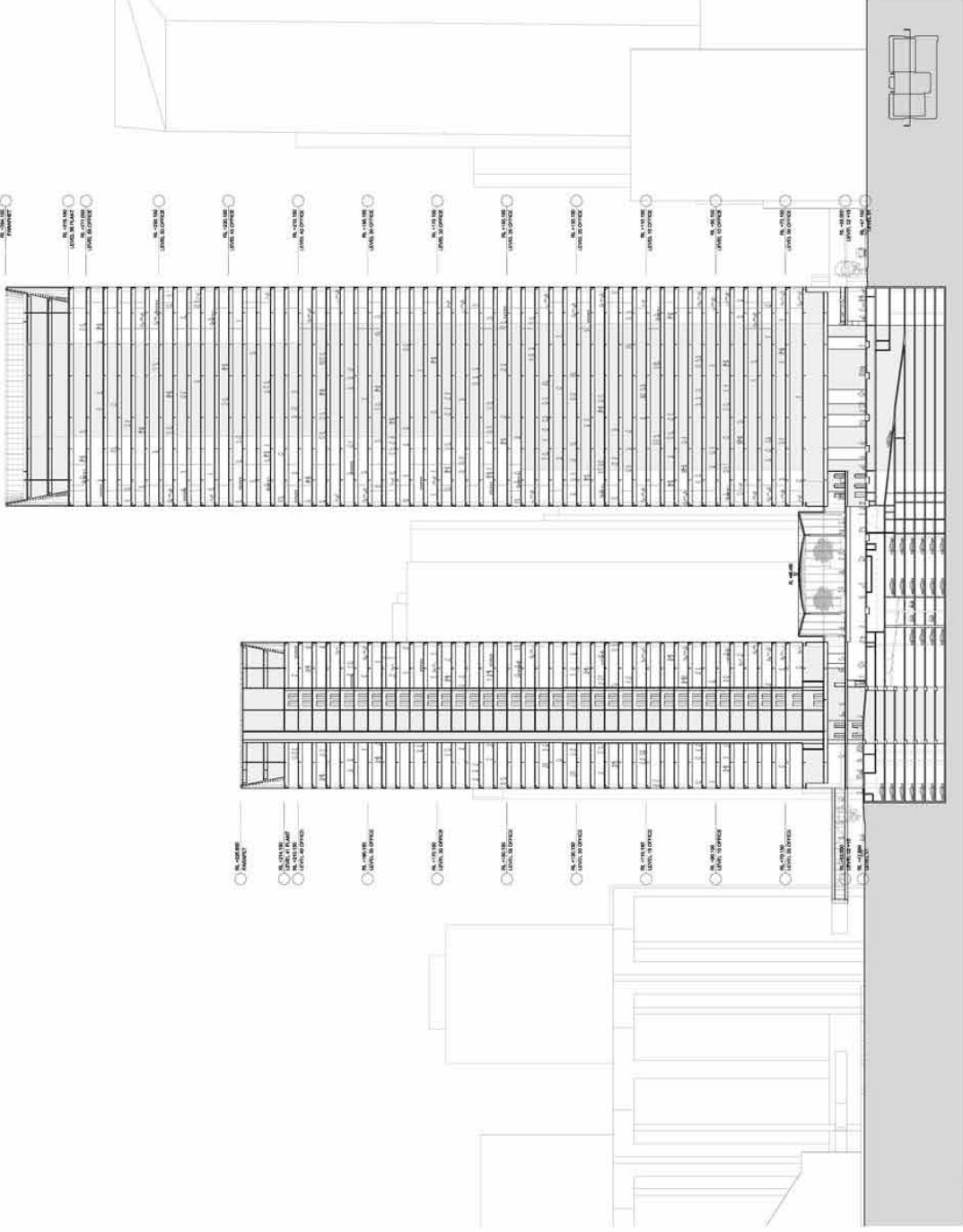
Brookfield

DIALOG 11 AVENUE E
SUITE 200
CALGARY, ALBERTA
T2C 0A5
P: 403.279.9541
F: 403.279.9541

WOODS BAGOT
DIAGNOSTIC HOUSE
1500 17TH STREET
WEDD 1B5
CALGARY, ALBERTA
P: 403.279.9541
F: 403.279.9541

64

DIALOG PROJECT NO. 080000000



WOODS BAGOT ARCHITECTS INC.

Community Association Comments



January 9, 2013

The City of Calgary
Development Circulation Controller
Development and Building Approvals #8201
P.O. Box 2100 Station M
Calgary AB T2P 2M5

Attn: Russ Mounty

Dear Russ:

Re: DP2012-4963 225 Sixth

The Calgary Downtown Association's (CDA) Urban Development Committee (UDC) has reviewed the above-mentioned development permit, and attended a presentation from Brookfield Properties Corporation and DIALOG.

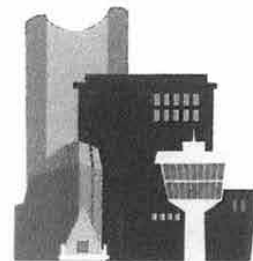
Below is a summary of our discussion. I trust it will be helpful in understanding the areas we are very supportive of, and where we have concerns.

- 1) The re-development of the entire block offers a wonderful opportunity to revitalize a significant area right in the core of the downtown. We are impressed by the transparency of the proposed towers as they are significant in height and would have had the potential to be very oppressive. The dramatic lighting of the top of the towers will be a positive addition to the downtown skyline.
- 2) The CDA firmly believes that the revitalization of this block will provide safety improvements in and around the area. With the increased population from the office towers in the area, and the proposed programming of both the outdoor plazas and internal public spaces, the experience of the pedestrian will be greatly enhanced. The public spaces have been well thought out, being very transparent and active, including the restaurant/café patio and kiosk proposed, and the two story pavilion complete with a casual gathering place similar to the Spanish steps in Rome. It will be important that non-slip, easily maintained pavement treatments be utilized for the plaza areas.
- 3) Retail space, proposed in both towers, is appropriate for the area, and will provide a number of much needed services and business opportunities.





- 4) Noise mitigation, with the proximity to the LRT tracks, will be paramount to the enjoyment of the pavilion and retail experience. We have been assured that this is being addressed with state-of-the-art materials. Patio spaces appear to be set back an appropriate distance to reduce the noise impact.
- 5) We are pleased that recognition of the significance of the Calgary Herald newspaper's contribution to downtown, in the form of public art, is being considered. In addition, we are encouraged by the number of opportunities for public art placements, and Brookfield's commitment to public art for their properties.
- 6) It is important in such a critical block in the downtown, that the number of parking stalls currently available is not reduced, so we are pleased that up to 1250 stalls will be available. We would encourage that as many of those stalls as possible be considered short-stay spaces.
- 7) The addition of 800 bicycle parking spaces and associated facilities, including dedicated bike access to the building, is welcomed.
- 8) Brookfield's commitment to LEED buildings is to be applauded, and we are pleased to see that this building will be a gold standard.
- 9) The CDA is very excited to see extremely well integrated +15 bridges proposed, including street access, balancing both levels for pedestrian movement. Having this block eloquently integrated with the existing +15 system will serve to streamline access in and around the property.
- 10) Access during construction in the area must be carefully managed. So far, during the demolition, the CDA has seen new standards set for the preservation of pedestrian access, while not sacrificing safety, and when road closures have been required, work has been done at night, to lessen the impact on traffic. This kind of planning and execution must be pervasive throughout construction.
- 11) We would encourage beautifying the construction hoarding for this project. It has been our experience that art installations significantly improved the pedestrian experience and reduce graffiti/tagging. The CDA would recommend engaging with Boardworx or other similar organizations to achieve the desired outcome.





The CDA is very pleased to endorse this project and land use changes required to facilitate it. We commend the efforts of those involved to earn their place in Calgary's skyline, as well as significantly contribute to the downtown experience for the general public.

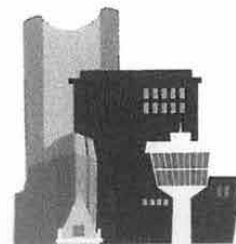
I trust our comments will be helpful. Please feel free to contact me at (403) 215-1565 should you wish to discuss this matter further.

Yours truly,

A handwritten signature in black ink, appearing to read 'M. A. Schofield'.

M. A. (Maggie) Schofield
Executive Director

cc: Mac Logan, General Manager, Transportation, City of Calgary
Ben Barrington, Manager, Centre City Implementation Team, City of Calgary
Inspector Grant, Calgary Police Service
Ian Parker, Senior Vice President, Asset Management of Brookfield Office Properties
Martin Sparrow, Principal, DIALOG
UDC



APPLICANTS SUBMISSION

Dear Mr. Mouny

**Re: 225 SIXTH - CPC Applicant Narrative
DP2012-4963**

Context

225 SIXTH is a new full-block commercial development in downtown Calgary located between 1st Street & 2nd Street and 6th & 7th Avenue SW. The proposed development of 2.8 million square feet represents a significant addition and urban redevelopment to the City Centre. The 57 and 42 storey towers are located at the northeast and northwest corners of the block respectively. Situated between the towers is a spectacular glazed urban pavilion of over 50,000 square feet, connecting the grade level open space to the Plus 15 system in an enclosed civic square. Bordered by lower historic buildings to the west and south, a major sunlit public plaza fronts 7th Avenue SW. This south facing plaza represents over one half acre of public open space. A connected smaller scale plaza at the SW corner of the block provides an intimate scaled urban space to the west tower and adjacent retail developments.

Siting

Locating the towers at the northeast and northwest corners of the block maintains a respectful distance between the historic buildings to the south, allowing for an enhanced pedestrian realm along 7th Avenue SW. The detailed design of the plaza emphasizes its urban context. Bosque's of trees define pedestrian scale and space within the plaza and provide definition of entry to the towers and pavilion. A freestanding retail kiosk in the plaza on the 1st Street edge continues the retail animation on the pedestrian edges and anchors the corner. A contiguous seat wall on 7th Avenue helps accommodate the subtle change in grade from 7th to 6th Avenues SW and provide containment for this pedestrian urban realm. The contemporary towers fit comfortably on the Calgary skyline while creating their own sense of place and visual address. The 57-storey East Tower will be the tallest structure in the downtown core.

Existing Buildings

Although none of the existing buildings on site are designated historic resources, a careful study of their history was conducted. Through this study it was determined that the façade of the Heagle Building would be catalogued, stored and ultimately reincorporated into the project. The Heagle is an example of 1950's modernist architecture and will be incorporated in a meaningful way, into the glass pavilion in Phase II.

The design of the new pedestrian plaza will acknowledge the history of The Calgary Herald newspaper as it occupied this particular site for over 20 years. As an example, we could envision that as part of the plaza design, the top 25 Calgary Herald Headlines over the last century would be set into the stone plaza paving as bronze word-art. This undertaking would be coordinated in parallel with our art program for the development.

Architecture

The contemporary modernist composition offers clarity of expression and a timeless, enduring character for this multi-tenant office tower, providing maximum transparency through a frameless low-iron clear glass skin. The buildings have a defined connection to the orthogonal street grain and gently taper and radius to a soft feathered connection to the sky plane. The lightness of character, transparency and

elegance of detail is set against an existing inventory of heavy precast, punched-window office towers in the downtown. The transparent structural glass pavilion touches the site lightly and sets itself away from the towers. It embraces the plaza and encloses an internal civic square, which will be programmed year round. It is a space that will perform well, whether it is programmed or not, from an informal coffee while sitting on the pavilion theatre steps to a gala dinner.

The Plaza

The Centre City Plan recognizes the importance of downtown open spaces like the proposed plaza, establishing the standard of a park or plaza within a five-minute walk for residents and employees. The plaza at 225 SIXTH faces south to take advantage of the sun year-round, acknowledging the 7th Avenue SW transit corridor. The plaza at over one-half acre, promises to be a significant, beautiful and effective addition to the evolving downtown public realm. It will be framed to the south by the Hudson's Bay Building, a juxtaposition of lower historic buildings against, this complementary significant public open space. Creating a dramatic arrival experience for those working in and visiting the new office tower, the plaza will see a steady stream of foot traffic, much of it headed to and from the LRT stop on 7th Avenue SW. The high building lobbies provides a visual connection to the plaza, transparency through the site and connection to 6th Avenue SW and the major destinations of The Core, Scotia Centre, Bow Valley Square and Suncor Energy Centre.

Retail Opportunities

Retail opportunities are provided along the west, south and east edges of the plaza, complementing the existing retail presence of the Hudson's Bay and providing protection from westerly winds. Cafés and restaurants are easily accommodated, with activity spilling out to animate the plaza; a satellite retail kiosk provides a public amenity in the urban plaza space. The Pavilion provides more than a connection to the plus 15 System by capturing and enclosing a civic square and folding the ground plane upward into a beautifully landscaped public dining terrace. The space is designed to be well used and appreciated by downtown employees and shoppers, coupled with significant programmed activity, public art and cultural events that will contribute to the vitality of the downtown throughout the week. This urban theatre is a metaphor for the historic Italian theater like La Scala by embracing the audience as part of the experience.

Landscape

Soft and hard landscaping complement the architecture and augment the function of the plaza. The design maximizes solar advantage, minimizes wind effects, accommodates pedestrian flows, reduces street noise impact, provides diverse places of animation and quiet and creates a place of visual delight in the downtown district. The buildings have been sited to protect from prevailing winds to ensure a comfortable space for pedestrians, while sitting or strolling. Urban groves of trees provide greenery, scale, shade and define activity and entrance. Stone paving provides an enduring and safe finish, seat walls and loose seating provide places for people, retail edges animate the space and careful consideration to site lighting provide security of use day or night.

225 SIXTH Highlights

- LEED Gold or Platinum certified.
- Fatal light awareness program for migratory birds.
- Enhanced sustainability and security (post 9/11) design features.
- Plaza is wind protected throughout the year.
- Plaza has sunlight throughout the year
- Minimal impact to existing vehicular traffic on 6th Avenue.
- Major programmed 50,000 square foot Urban Pavilion.
- 40,000 square feet of creative food and retail components to animate the development.
- Over one-half acre of landscaped open space open to the sky.

- Creative food experience with seating for over 900 people.
- New, +15 links to Scotia Centre and Bow Valley Square and Energy Plaza.
- Building population of over 8000.
- Security, lighting and safety integral to design.

Sustainable Design Principles

Brookfield, has committed that all their new developments will be sustainably designed and that all new buildings (globally) are to achieve LEED Gold certified status (or higher). Brookfield's two most recent developments in Canada, Bankers Court in Calgary and Bay Adelaide Centre in Toronto, both were certified LEED Gold. With this goal in mind, 225 SIXTH Avenue has been designed to achieve LEED Gold (or higher) through the LEED Canada 2009 Core and Shell rating system. The building incorporates several sustainable features throughout which will contribute to the LEED Gold rating.

The building is situated in a densely populated urban site that is well served by public transportation. Thus the location of the project itself lends well to achieving LEED credits. It will prevent light pollution on neighboring public spaces and properties and will allow the project to manage and retain storm water onsite via a cistern.

The extensive landscaping in the plaza will incorporate water efficient irrigation and drought resistant plant species. In addition, the building will use low flow fixtures to reduce water use in the tower. Captured rainwater will be used for irrigation. The building will use an ultra high efficiency curtain wall to reduce heating and cooling needs in the building along with condensing boilers and high efficiency chillers to reduce the energy use of the development.

The roof system on the tower will be a highly reflective polymer membrane, which will reduce the heat island effect of the tower. Light colored pavers and landscaping reduce the potential heat island effect at the ground plane. The existing buildings on site will be extensively recycled, along with construction waste, helping to divert waste from Calgary's landfills. Materials with a high-recycled content will be incorporated in the project. In addition, construction materials will be regionally sourced, reducing the environmental cost of transportation.

Low emitting finishes will be specified for the project, contributing to a healthier work environment. In addition, there will be daylight and views for a large portion of the regularly occupied spaces, reducing the need for artificial lighting and creating a bright and open space. An educational outreach program will highlight the sustainable features of the building and teach the occupants how they can reduce their environmental footprint. A green housekeeping program will also be implemented, which will help to ensure the building is maintained in a healthy and environmentally friendly way.

Bird Friendly Urban Design Guidelines

In response to the Bird Friendly Urban Design Guideline and Centre City Illumination Guidelines a list of strategies outlining our design response is as follows:

In order to make the lower portion of the tower more bird friendly, the lower two storeys are clad in clear non-reflective glazing. In addition, all vegetation and landscaping is located away from the building. These two tactics will help to reduce reflections of landscape on the glass. There is no interior landscaping near windows, which will prevent the suggestion of desirable habitat to birds on the outside of the building.

To make the tower more bird friendly, glazing will be interrupted with spandrels. The use of blinds though out the tower will be used to make the clear glass more opaque. The blinds will also help to interrupt the glazing so it will not appear monolithic. These measures will prevent birds from seeing reflective glass as either open sky or inviting habitat.

The use of occupancy sensors on lighting will be used in the tenant spaces in order to reduce the hours, which the building is illuminated thus reducing light pollution. Site lighting will be limited to what is necessary for safety and urban vitality. This will help to prevent birds from becoming lured and trapped from city lights.

Centre City Illumination Guidelines

A microprocessor-based lighting control and energy monitoring system will be provided to control all lighting in the building and occupancy sensors will be provided throughout the typical floor areas. This system will help to reduce energy consumption. In addition, high efficiency and high color rendering lamps that control light pollution will be used throughout the building and in the outdoor spaces. Sidewalks and streets will be lit to IESNA standards.

The more intimate pedestrian spaces will be lit such that the occupants feel safe and are aware of changes in the landscape. Overall it is important that the space feels lively in the evening, as this will provoke a sense of security and safety. This is achieved by various lighting strategies throughout the plaza and around the building. The plaza lighting will focus on reinforcing the various zones that the landscape is divided into while minimizing the view of light sources and avoiding light trespass. The lighting along benches, steps and handrails will enhance site details while providing ambient light. Additional elements such as trees and sculpture will be uplighted with low output in-ground fixtures to identify the solid elements in the space while avoiding excessive light pollution. These measures will help to contribute to a bird friendly urban environment.

If you have any questions on 225 SIXTH, please don't hesitate to contact me at 403-541-5406.

Sincerely,



Martin Sparrow, M. Arch, MBA, Architect AAA, OAA, AIBC, NWTAA, MRAIC, **LEED® AP**
Principal

UDRP Comments	Applicant Response
<p>The Panel commends the Applicant for a thoughtfully conceived and articulated design concept. As this project encompasses an entire city block, without providing street-wall/street-edge definition in a prominent downtown location, the panel feels that it is important that the final resolution and execution of detailing, particularly at street level, is well considered to ensure a strong contribution to the urban realm.</p>	<p>The applicant agrees with this position and will continue to develop the design accordingly.</p>
<p>The Panel notes the importance of the location and use of the kiosk in the Southeast corner of the plaza and feels that its design and detailing must be exceptional. The panel requests that additional architectural drawings which detail its design, materials, finish, and lighting as well as an operational and servicing protocol be provided.</p>	<p>The kiosk will be serviced across the plaza via the service elevator located in the pavilion. Incoming provisions and outgoing trash and recycling will not be staged on the plaza, ensuring it remains clutter free and undamaged. Appropriate waste and recycling bins will be provided and carefully integrated into the plaza design. It is anticipated that the kiosk will provide an all-day offering, with coffee and breakfast items in the AM and light lunch and dinner items and beverages during the remainder of the day. The kiosk may be associated with one of the interior restaurants. It is anticipated that patrons will make their selection and receive their items at a walk up counter, not through table service. All building services, such as mechanical, electrical and plumbing will be provided from below. Moveable heaters will be provided in the adjacent seating</p>
<p>The Panel notes that the power of the minimalist design rests on the quality of the buildings' interface with the public realm. While the Applicant has carefully and satisfactorily resolved the design of the building envelope above the ground plane, the public interface is largely dependent on the selection of materials and the design of their details where the building envelope meets the ground plane. The Panel requests that additional architectural details explaining the materials, glazing, paving and drainage be provided particularly at the scale and height of the pedestrian.</p>	<p>The applicant agrees with this position and will continue to develop the design accordingly.</p>
<p>The panel is concerned that the interface between the plaza and interior spaces has not been fully explored or detailed to ensure the objective of a park like interior is achieved as well as a strong connection between inside and outside. The panel request further clarification on the location and types of doorways proposed.</p>	<p>The pavilion is conceived as an active, vibrant interior urban piazza modeled on successful examples within the Brookfield portfolio and elsewhere. It is conceived as a new urban centre for Calgary, capable of hosting significant civic events and exhibitions. It is also understood that it must enrich the urban environment during times when there are no events or exhibitions ongoing. A delicate balance needs to be reached between these aspirations. It will be animated with cafes and restaurants along its edges with movable public</p>

seating spilling out into the interior. During the many events that will be held there, the movable seating will be either rearranged or removed as appropriate. Free Wi-Fi and ample public seating will be provided which, combined with the rich variety of food and beverage offerings will create a very 'sticky' place. The pavilion is located at the heart of the +15 system and directly midpoint between two LRT stops, ensuring that a steady stream of Calgarians will use and animate it throughout the day and evening. The stairs from the ground to +15 are deliberately modeled, in scale and proportion on the Spanish Steps in Rome ensuring they will become the seating from which to enjoy the urban theatre that will unfold within the space.

The interior of the pavilion will be a dynamic, social environment: confirmed through specific elements devised to animate the space,

Main Floor

1. 'Spanish Steps': A graceful connection between ground and plus 15 levels, deliberately modeled, in scale and proportion on the Spanish Steps in Rome, ensuring they will become the seating from which to enjoy the urban theatre that will unfold within the space.
2. Public Seating: This area forms the 'piazza' immediately beneath the 'Spanish Steps', and provides a zone for performances and people watching, or public seating and loose moveable furniture for groupings that can be adapted by the public. We are continuing to look for opportunities to add greenscaping elements within these zones, which may be moveable.
3. Sidewalk Cafe: The restaurants and cafes that front this activity zone will open directly onto this space and alfresco seating provided to activate the pavilion. We are currently exploring the possibility of integrating greenscape elements within the sidewalk cafe areas.
4. Service Retail Arcade: The arcade provides a connection across the entire development and takes advantage of high pedestrian traffic volumes with strategically located retail to service the population.

	<p>+15 Level</p> <ol style="list-style-type: none"> 5. Promenade: The plus 15 walkway is transformed into a viewing gallery as it opens up to the generous pavilion interior, allowing its occupants the opportunity to soak up the ambience. The promenade is flanked by cafe-style food offerings and seating areas, encouraging people to linger. 6. Sidewalk Cafe: The pavilion will become a destination for cultural activity, as well as introduce a new concept in retail food offering which is unrivalled in the City. Cafe-style and bar seating are positioned to front onto circulation spaces, to co-mingle the activities and generate social energy. We are currently exploring the possibility of integrating greenscape elements within the sidewalk cafe areas. 7. Sidewalk: In addition to a primary means of circulation, activated frontages will encourage people to migrate between different areas of activity. The sidewalk is flanked by cafe-style food offerings and seating areas, encouraging people to linger. 8. Grove Dining / Public Seating: This occupies an area within an allee of tall trees. The space is flexible and could support an evening gala or formal ceremony; public seating and loose moveable furniture for groupings that can be adapted by the public may be incorporated. The space can be enhanced through artwork that engages and inspires the public and serves as an attractor for people within the +15 network.
<p>The Panel recommends removal of the steps outside the building within the public plaza to facilitate the complete integration of indoor and outdoor space between the plaza and the winter garden. In addition the creation of a space going down to the north from the raised "main front porch" of the building entry will be uninviting. It's success will be dependent on a savvy retailer to activate that depressed area and it is better the success of that corner depends on the urban design rather than the right tenant selection</p>	<p>Although gentle, there is a significant grade change across the site from north to south that must be managed. It was deemed important to provide access to the pavilion directly off 7th Avenue without grade change. The stairs serve to mediate the grades, support sight lines and enhance circulation, negating rails and retaining walls that would be required if the stairs were eliminated. Additionally it was deemed important to provide access to the east tower lobby directly off the plaza without grade change. These objectives, combined with a desire to limit the grade change from 6th Avenue to the east tower lobby and the imperative to eliminate grade change from sidewalk to plaza anywhere along their interface, necessitated a slight grade change from</p>

	<p>the plaza to the pavilion at its east entrance portal. This grade change is accomplished with very gentle 'feathered' steps with the greatest total rise not exceeding 700mm. The space north of these steps will be furnished with moveable furniture for patrons of the adjacent café.</p> <p>The porch is low and shallow and will serve programmatic activities of the site: a subtle stage for performances, audience seating for events in the plaza, a social eddy space that creates an active edge to the plaza. The corner to the north of the porch and stairs will be well lit with clear views in and out.</p>
<p>The Panel recommends that the vehicle access on 6 Ave be reduced through the addition of an access on 2nd Street, to reduce the overall impact of vehicles on the pedestrian realm of 6th Av. The activation of that face of the building is not well considered and the panel suggests that one entry point on 6th Avenue and one on 1st Street will improve this condition.</p>	<p>The design team has worked closely with The City and our transportation and traffic consultants and clearly determined that access for parking and loading can only reasonably be accessed from 6th Avenue. Additionally, an access on 2nd Street would significantly impact the pedestrian oriented aspirations for this street.</p>
<p>The Panel recommends a significant and direct visual and pedestrian access be created from 6th Av to the winter garden to enhance the vitality of 6th Av. The panel feels that more pedestrian entrances and improved permeability on 6th Av will generally improve the pedestrian character along this street.</p>	<p>The design team has identified the major pedestrian movements along 6th Avenue as generating from the 1st and 2nd Street directions, not from the centre of the block. Escalators and stairs from ground to +15 level have been provided in each lobby immediately adjacent to, and highly visible through, the 6th Avenue lobby facades. Moving from either the east or west direction, pedestrians moving to the pavilion will have a highly visible and intuitive route to the +15 level and pavilion through the office lobbies. The experience for pedestrians moving along the full block between 1st and 2nd Streets will be enlivened and animated by the quality, character and significant scale of the lobbies. In addition, access to the 600 bike spaces and associated facilities is located at the midpoint of the block, ensuring this area will be animated by an estimated 1000 trips per day.</p>
<p>The Panel feels that the indoor park spaces are not adequate in terms of vegetation and landscaping. The panel requests an expanded and comprehensive landscaping plan to enhance the quality of the indoor parks.</p>	<p>The pavilion is conceived as an active, vibrant interior urban piazza modeled on successful examples within the Brookfield portfolio and elsewhere. It is conceived as a new urban centre for Calgary, capable of hosting significant civic events and exhibitions. It is also understood that it must enrich the urban environment during times when there are no events or exhibitions ongoing. A delicate balance needs to be reached between these aspirations. It will be animated with cafes and restaurants along its edges with movable public seating spilling out into the interior. During the many</p>

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+15 Level

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the opportunity to soak up the ambience. The promenade is flanked by cafe-style food offerings and seating areas, encouraging people to linger.

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Grove Dining / Public Seating: This occupies an area within an allee of tall trees. The space is flexible and could support an evening gala or formal ceremony; public seating and loose moveable furniture for groupings that can be adapted by the public may be incorporated. The space can be enhanced through artwork that engages and inspires the public and serves as an attractor for people within the +15 network.

Direct Control District Guidelines

1 Purpose

This Direct Control District is intended to:

- (i) allow the comprehensive redevelopment of the block, bounded by 6 and 7 Avenues SW between 1 and 2 Streets SW in accordance with the aims of the Centre City Plan;
- (ii) provide for the replacement of the existing 1038 parking stalls within the Bow Parkade;
- (iii) provide for additional density bonus standards that provide public benefits.

2 Compliance with Bylaw 1P2007

Unless otherwise specified, the rules and provisions of sections 1-4 of Part 1, sections 21(1), (2) and 22 of Part 2 and Part 10 of Bylaw 1P2007 apply to this Direct Control District.

3 Reference to Bylaw 1P2007

Within this Direct Control District, a reference to a section of Part 10 of Bylaw 1P2007 is deemed to be a reference to the section as existed on the date of the passage of this Direct Control District Bylaw.

4 General Definitions

In this Direct Control District,

- (i) "bicycle parking stall" means an area approved as bicycle parking stall – class 1 or bicycle parking stall – class 2 that is equipped to store a bicycle and must include a device:
 - (a) specifically designed to park a bicycle;
 - (b) designed to allow a bicycle frame and both wheels to be secured; and
 - (c) designed to support the bicycle frame and both wheels; and
 - (d) that is anchored to a hard surface or fixed structure.
- (ii) "bicycle parking stall – class 1" means a bicycle parking stall in a secured or controlled area.
- (iii) "bicycle parking stall – class 2" means a bicycle parking stall in an unsecured or uncontrolled area.

5 Permitted Uses

The following uses are permitted uses in this Direct Control District:

Home occupations - Class 1
Power Generation Facility, Small-scale
Signs - Class 1
Special function - class 1

Notwithstanding any other requirement of this Bylaw, proposed or existing uses of a site shall be permitted uses on that site if they:

- (a) are included in the list of Discretionary Uses in Section 42.3(4) of Part 10 of Bylaw 1P2007; and

- (b) have been approved on or before October 10, 1984, by a development permit that has not expired.

6 Discretionary Uses

The following *uses* are *discretionary uses* in this Direct Control District:

- Accessory food services
- Amusement arcades
- Automotive specialties
- Billiard parlours
- Child care facilities
- Commercial schools
- Drinking establishments
- Dwelling units
- Entertainment establishments
- Essential public services
- Financial institutions
- Gaming establishment - bingo
- Grocery stores
- Hotels
- Laboratories
- Liquor stores
- Mechanical reproduction and printing establishments
- Medical clinics
- Offices
- Outdoor cafes
- Parks and playgrounds
- Parking Area – short stay
- Parking Structure
- Personal service businesses
- Private clubs and organizations
- Private schools
- Public and quasi-public buildings
- Public and separate schools
- Radio and television studios
- Restaurants-food service only
- Restaurants-licensed
- Retail food stores
- Retail stores
- Signs – Class 2
- Special function - class 2
- Take-out food services
- Universities, colleges, and provincial training centres
- Utility Building
- Veterinary clinics

7 Bylaw 1P2007 District Rules

Unless otherwise specified, the rules of the CM-2 Downtown Business District of Part 10 of Bylaw 1P2007 apply in this Direct Control District.

8 Location of Uses

(a) The following uses must not be located on the ground floor of a building:

- Billiard parlours
- Commercial schools
- Dwelling units
- Laboratories
- Offices
- Private clubs and organizations
- Private schools
- Public and quasi-public buildings
- Public and separate schools
- Universities, colleges, and provincial training centres

(b) In addition to the uses listed in subsection (1) above, the following uses must not be located on the second floor of a building when the building is connected to the +15 System:

- Automotive specialties
- Entertainment establishments
- Essential public services
- Gaming establishment - bingo
- Radio and television studios

9 Required Motor Vehicle Parking Stalls

(a) The requirements of Section 18 of Part 10 of Bylaw 1P2007 apply to this Direct Control District, with the exception of section 18 (1.1)(a)(iii).

(b) Notwithstanding the requirements of subsection 18 (c) of Part 10 of Bylaw 1P2007, the parcel may be redeveloped to provide an additional 1038 parking stalls.

(c) Any of the additional 1038 parking stalls referred to in subsection (2) that are not provided within the development site may be used as credit against that portion of the required parking stalls for the development that is required to be paid as cash-in-lieu under Section 18 (1.1) (b) OF Part 10 of Bylaw 1P2007.

(d) A minimum of 15 percent of the parking stalls provided on the parcel must be provided within the development site as parking area – short stay.

(e) The parking stalls provided within the parking area – short stay must be:

- (i) Located in a portion of the development approved for use as a parking area - short stay with convenient public access to street level and adjoining publicly accessible uses
 - (ii) Identified through appropriate signage as parking area – short stay; and
 - (iii) Prominently signed at the street level indicating the availability and conditions of use of such stalls
- (f) The provision of the required parking stalls and loading spaces for the development may be phased, providing a phasing plan is submitted to and approved in writing by the Approving Authority.

10 Required Bicycle Parking Stalls

- (a) For an Office, the minimum number of required:
 - (i) bicycle parking stalls - class 1 is 1.0 per 1000.0 square metres of net floor area; and
 - (ii) bicycle parking stalls - class 2 is 1.0 per 1000.0 square metres of net floor area.
- (b) For a Dwelling Unit or Live Work Unit, the minimum number of bicycle parking stalls – class 1 is 1.0 per unit for developments with greater than 20.0 units.
- (c) Required bicycle parking stalls – class 1 must be located at grade or within the first parkade level directly below grade.
- (d) The provision of the bicycle parking stall requirement for the site may be phased.

11 Discretionary Use Rules Gross Floor Area

All development, regardless of density, shall provide all Bonus Group A features of the Bonus Density Table to the satisfaction of the Approving Authority. The maximum gross floor area, calculated using the Bonus Density Table, shall be

- (i) 7 F.A.R., which shall not be refused on the grounds of density only, where all Group A features of the Bonus Density Table are provided to the satisfaction of the Approving Authority;
- (ii) 15 F.A.R. where, in addition to provision of all Group A features, Group B features of the Bonus Density Table of a type, location, and design required by and acceptable to the Approving Authority are provided;
- (iii) 20 F.A.R. where, in addition to provision of all Group A features, Group B and Group C of the Bonus Density Table of a type, location, and design are provided in accordance with Council's policy for public improvements in the Downtown.

12 Density Bonus Table

The Density Bonus Table for this Direct Control District Bylaw forms part of this bylaw.

BONUS DENSITY TABLE LOC2012-0055

BONUS GROUP A. A maximum of 7 F.A.R. may be achieved through provision of group A features.

STANDARD	PUBLIC AMENITY TO BE PROVIDED	LOCATION AND ACCESS REQUIREMENTS	PERFORMANCE REQUIREMENTS	ILLUSTRATION
A1	At-Grade Pedestrian Circulation (a) On-Site Pedestrian Space (b) Street Corner Pedestrian Space	<ul style="list-style-type: none"> Abutting primary roads, the 7 Avenue S. and future L.R.T. alignments and the secondary roads of 1 and 2 Streets S.W., all as shown on Map 1, Roadway Network. Directly accessible to and level with public sidewalks. At all intersections. Directly accessible to and level with public sidewalks. 	<ul style="list-style-type: none"> If arcaded, a minimum unobstructed width of 3.5 metres. If no structure to grade, a minimum unobstructed width of 2.2 metres from the setback line along primary roads and L.R.T. Corridors or 1.5 metres along 1 and 2 Streets S.W. Open to sky or built-over above second storey. A triangular area formed by the two setback lines and a straight line which intersects them 7.5 metres from the corner where they meet. Open to sky or built-over above second storey. No vegetation, finished lot grade, building or structure within the triangular area formed by the two setback lines and a straight line which intersects them 3 metres from the corner where they meet. Buildings, signs, columns permitted to satisfaction of the Approving Authority. Clearly and appropriately signed for pedestrians. A minimum unobstructed width of 4.5 metres. Pedestrian path shall be a minimum of 5.5 metres above grade. Capable of accepting +15 bridges and lane links. A minimum unobstructed width of 2.0 metres. May be indoors or outdoors. Clearly visible and directly adjacent and accessible to street or avenue sidewalk and +15 system. In accordance with approved policy. 5 percent of net site area. Open to sky. Depth of open space may not exceed 1.5 times the street or avenue frontage of such open space. 	
A2	+15 System (a) Development must make provision for connecting to the +15 System by: (i) Walkways (See also Bonus B7b) (ii) Supports (iii) Vertical Movement Between Grade and +15 Level (b) Contribution to +15 Fund	<ul style="list-style-type: none"> Where required by the Approving Authority, routes shall be oriented in north/south and east/west directions, continuous with existing and potential +15 routes on neighbouring sites. Within the net site area. Where required by the Approving Authority. Must provide for public use either interior stair, ramp, escalator or elevator between grade and +15 levels. One elevator must provide access to both the grade and +15 level. Where a +15 bridge is to be located adjacent to the site, a means of vertical movement (indoors or outdoors) shall be in a location convenient to the +15 bridge. 	<ul style="list-style-type: none"> Clearly and appropriately signed for pedestrians. A minimum unobstructed width of 4.5 metres. Pedestrian path shall be a minimum of 5.5 metres above grade. Capable of accepting +15 bridges and lane links. A minimum unobstructed width of 2.0 metres. May be indoors or outdoors. Clearly visible and directly adjacent and accessible to street or avenue sidewalk and +15 system. In accordance with approved policy. 	
A3	Open Space At Grade	<ul style="list-style-type: none"> Abutting street or avenue. 	<ul style="list-style-type: none"> 5 percent of net site area. Open to sky. Depth of open space may not exceed 1.5 times the street or avenue frontage of such open space. 	

See Section 42.3 (5) (a) (ii) (C) for special exemption for public auditoria, cinemas and theatres.
 See Section 42.3 (5) (a) (ii) (D) for special exceptions for the development of sites of 3,020 square metres gross site area or less. 13P87

BONUS DENSITY TABLE

BONUS GROUP B: A maximum additional 8 F.A.R. may be achieved through provision of Group B features.

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS RATIO DEVELOPMENT : FEATURE AREA	LOCATION AND ACCESS	PERFORMANCE REQUIREMENTS	ILLUSTRATION
B1	At-Grade Plaza (Excludes any Standard A1 and A3 spaces)	Plazas 250 square metres to 499 square metres in size - 10:1 Plazas 500 square metres or over in size - 15:1 Maximum of 20 percent of gross site area eligible for bonus density. Where plazas abut north/south streets, an additional 25 percent bonus for that portion of the plaza where the north/south street frontage equals or exceeds the depth.	<ul style="list-style-type: none"> At-grade or within millimetres above or below grade with no wall along the sidewalk higher than 450 millimetres. Direct access to sidewalk or on-site pedestrian space amounting to at least 50 percent of each of the plaza's street and avenue frontage. Retail or personal service units abutting a plaza shall front onto it and have individual direct plaza level access to it. 	<ul style="list-style-type: none"> Open to sky. A minimum contiguous area of 250 square metres. A minimum dimension of 9 metres in all directions. A minimum frontage along street or avenue of 9 metres, with that frontage equal to or greater than the plaza depth. The combined width of all entrances and lobbies of office developments shall not exceed 15 percent of the perimeter distance of all building fronting onto the plaza, or 10 metres whichever is the greater. A minimum ratio of 1 seat or 750 millimetres of bench seating for each 10 square metres of plaza area. 	
B2	Space: <ul style="list-style-type: none"> Other At-Grade (Excludes any Standard A3 spaces) Standard A1 space eligible for this bonus where open to sky. 	7.5:1	<ul style="list-style-type: none"> At-grade or within millimetres above or below grade, with no wall along the sidewalk higher than 450 millimetres. Accessible and visible from public sidewalk or on-site pedestrian space. 	<ul style="list-style-type: none"> Open to sky. Depth of open space may not exceed 1.5 times the street or avenue frontage. Other at-grade, open to the sky areas (A1, A3, B1) must be included in the measurement depth. 	
B3	Sloped or Terraced Open Space	5:1	<ul style="list-style-type: none"> Between grade and +15. Physically accessible to, and visible from, sidewalk or on-site pedestrian space, and where required by the Approving Authority, from the +15 walkway. 	<ul style="list-style-type: none"> Open to sky. A minimum dimension of 6 metres in all directions. 	

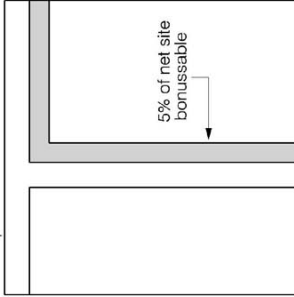
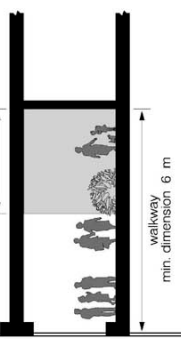
BONUS DENSITY TABLE

BONUS GROUP B: Continued

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS RATIO DEVELOPMENT : FEATURE AREA	LOCATION AND ACCESS	PERFORMANCE REQUIREMENTS	ILLUSTRATION
B4	Open Space at +15 Level	4:1	<ul style="list-style-type: none"> At +15 level. Visible from +15 walkway for its whole length where the walkway abuts the open space. Direct access from walkway to abutting open space at least every 9 metres. 	<ul style="list-style-type: none"> Open to sky. A minimum width of 6 metres in all directions. Frontage of open space along the +15 walkway must be greater than the depth of the open space. 	
B5	Outdoor Built-Over Space Standard A1(a) space arcaded or built over is eligible for this bonus.	4:1	<ul style="list-style-type: none"> Directly accessible to and level with public sidewalk, on-site pedestrian area and/or at-grade open space. 	<ul style="list-style-type: none"> Built over above second storey but not enclosed. If arcaded, a minimum unobstructed width of 3.5 metres up to a maximum of 5 metres with a minimum contiguous area of 30 square metres. If no structure to grade, a maximum unobstructed width of 5 metres. 	
B6	Indoor Park	Where provided at grade level - 12.5:1 Where provided at +15 level - 10:1 Maximum of one third of gross site area eligible for this bonus.	<p>At-Grade</p> <ul style="list-style-type: none"> Must front on, have direct access to, and be visible from the sidewalk, grade level open space or on-site pedestrian space. Must be a distinctly separate area from lobby or reception area. <p>+15 Level</p> <ul style="list-style-type: none"> Must front on and be visible from the street or avenue. Must have direct access to street or avenue by indoor stairs, escalators or elevators. Directly accessible to, and visible from, +15 walkway. For the whole length of the +15 walkway where it abuts the indoor park, it must be glazed, with direct access between the walkway and the park at least every 9 metres, or else completely open without barriers. 	<ul style="list-style-type: none"> A minimum width of 9 metres and a minimum contiguous area of 150 square metres, excluding access ways to elevators and required +15 walkways. Exterior walls shall be glazed except where they abut another property. Minimum 8 metres vertical clearance between floor and non-glazed roof or ceiling and 4.5 metres between floor and glazed roof. Intensively landscaped to create a park-like setting which must include seating, and may include playgrounds and performance areas. A minimum of 50 percent of the area of the park shall have a glazed roof. The depth of any area of the park without a glazed roof shall not exceed 8 metres measured from the point where it abuts the area with a glazed roof. Where a park area without glazing above is separated from an exterior wall by a glazed-over area, the depth of that area without glazing above shall not exceed the distance that it is separated from the exterior wall. Where a +15 walkway covers a portion of at-grade indoor park, the area directly under the walkway which may not exceed 4.5 metres in width, is eligible for a B6 bonus only where that walkway is separated from the exterior glazed walls surrounding the park by a distance of not less than 4.75 metres. No more than 15 percent of the indoor park may be covered by a +15 walkway. 	

BONUS DENSITY TABLE

BONUS GROUP B: Continued

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS RATIO DEVELOPMENT : FEATURE AREA	LOCATION AND ACCESS	PERFORMANCE REQUIREMENTS	ILLUSTRATION
B7	<p>+15 Walkway Enhancement</p> <p>(a) Maximum of 5 percent of the net site area, excluding area of basic on-site north/south and east/west +15 walkway connections required as in Standard A2, with the following exception.</p> <p>(b) Where a required A2 walkway exceeds 20 percent of the net site area, the amount by which it exceeds that 20 percent is eligible for this bonus. If this amount exceeds 5 percent of the net site area, the B7(a) bonus is not available. If the amount is less than 5 percent, a total of 5 percent may be bonussed as B7(a) and B7(b).</p>	4:1	<ul style="list-style-type: none"> At +15 level. Directly accessible to the +15 system. 	<ul style="list-style-type: none"> A minimum unobstructed width of 6 metres which may be measured in combination with the +15 walkway required in Standard A2. Widened walkways shall provide landscaping, seating areas, information and other public facilities. 	
B8	<p>+15 Bridge</p> <p>(a) Standard Bridge Sites may build or contribute to the system the equivalent of 1 bridge from an area of up to and including 3020 square metres; 2 bridges from an area of over 3020 square metres but not more than 6040 square metres; 3 bridges from an area of over 6040 square metres but not more than 9060 square metres; and 4 bridges from an area over 9060 square metres. Extra Skylighting</p> <p>(b) Where more than 50 percent of a bridge is skylit, the whole skylit portion will be bonussed at 2:1.</p>	20:1 Calculated on bridge floor area over right-of-way.		<ul style="list-style-type: none"> A minimum clearance of 4.75 metres except a minimum clearance of 6 metres over the L.R.T. Corridors. A minimum unobstructed width of 4.5 metres. A maximum unobstructed width of 6 metres eligible for bonus density. Magnetically controlled doors. No air conditioning or other equipment shall be located on the roof of the bridge. Minimum of 75 percent of total wall surface clear glazed between 0.5 and 2.5 metres above bridge floor for the total length of the bridge. More than 50 percent of floor area with transparent glazing directly above. 	

BONUS DENSITY TABLE

BONUS GROUP B: Continued

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS RATIO DEVELOPMENT : FEATURE AREA AREA	LOCATION AND ACCESS	PERFORMANCE REQUIREMENTS	ILLUSTRATION
B9	Provision of Pair of Escalators	30:1 Calculated on area of escalator's floor plate. Maximum of 1 F.A.R.	<ul style="list-style-type: none"> Directly connects public access areas at-grade to those at +15 level. 	<ul style="list-style-type: none"> A minimum unobstructed width of 6 metres which may be measured in combination with the +15 walkway required in Standard A2. Widened walkways shall provide landscaping, seating areas, information and other public facilities. 	
B10	Lane Link Sites may build or contribute to the system the equivalent of 1 link from an area up to and including 3020 square metres; 2 links from an area over 3020 up to and including 6040 square metres; 3 links from an area over 6040 up to and including 9060 square metres; 4 links from an area over 9060 square metres.	12.5:1 Calculated on area of link over lane right-of-way.		<ul style="list-style-type: none"> A minimum unobstructed width of 4.5 metres. A maximum unobstructed width of 6 metres eligible for bonus except a maximum of 10 metres where the lane link abuts a bridge. A minimum clearance of 4.75 metres above grade with the elevation of the pedestrian path a minimum of 5.5 metres above grade. 	
B11	Improvements to Adjacent Right-of-Way (a) Street Enhancement (b) Mall Enhancement	2.5:1 2.5:1	<ul style="list-style-type: none"> Sites abutting streets or avenues. Area limited to frontage of the site. Sites abutting Stephen (8th) Avenue Mall or Barclay (3rd Street) Mall. 	<ul style="list-style-type: none"> Enhanced treatment and materials over and above basic standards for sidewalk reconstruction consistent with Council's policy for public improvements in the Downtown. Area limited to the frontage of the site. Mall construction or enhancement consistent with Council's policy. Area limited to the frontage of the site by the full width of the mall. 	

BONUS DENSITY TABLE

BONUS GROUP B: Continued

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS RATIO DEVELOPMENT : FEATURE AREA	LOCATION AND ACCESS REQUIREMENTS	PERFORMANCE REQUIREMENTS
B12	<p>Scripture in Public Spaces</p> <p>(a) Provided on Site</p>	<p>1 square metre of floor area for every \$110 (October 1984 dollars) of sculpture provided. Maximum of 1 F.A.R. 13P87</p>	<ul style="list-style-type: none"> Setting acceptable to Approving Authority on site. If off the development site, on the Stephen (8th) Avenue Mall, Barclay (3rd Street) Mall or a Downtown public park. Minimum of 75 percent of total value contributed to sculpture used outdoors at-grade or on building exterior visible from the sidewalk. Maximum of 25 percent of total value contributed to sculpture used in indoor park. 	<ul style="list-style-type: none"> Unique not mass-produced sculpture by a practitioner in the visual arts who is generally recognized by critics and peers as a professional of serious intent and ability. Value of sculpture provided calculated in accordance with Council's approved policy. In accordance with Council policy.
B13	<p>Sites Retaining Heritage Features</p>	<p>Up to a maximum of 1 F.A.R. of the site originally covered by the Heritage Building.</p>		<ul style="list-style-type: none"> Amount of density to be determined by the Approving Authority based on importance of retained feature, cost of retention and relative size of heritage feature. Features of heritage significance to be retained and integrated into the new building to the satisfaction of the Approving Authority.
B14	<p>Sites Receiving Density from Heritage Buildings</p>	<p>Up to a maximum of 3 F.A.R., except on sites with a net site area of 1812 square metres or less where this limit may be exceeded.</p>		<ul style="list-style-type: none"> Available only in accordance with Council policy where density is being transferred from another site, as per Section 42.3(5)(f), or by purchase from a heritage preservation fund.
B15	<p>(a) Sites Receiving Density from the Density Transfer Area - Stephen (8th) Avenue Mall</p> <p>(b) Sites Receiving Density as a result of the protection of sunlight on Public Spaces</p>	<p>Up to a maximum of 3 F.A.R., except on sites with a net site area of 1812 square metres or less where this limit may be exceeded.</p>		<ul style="list-style-type: none"> Available only in accordance with Council policy where density is being transferred from another site, as per Section 42.3(5)(f). Available only in accordance with Section 42.3(5)(i).

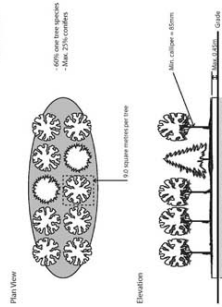
BONUS DENSITY TABLE

BONUS GROUP C: Once a density of 15 F.A.R. is achieved through the provision of Group A and B features, a maximum additional 5 F.A.R. may be achieved through provision of Group C.

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS	LOCATION AND ACCESS REQUIREMENTS	PERFORMANCE REQUIREMENTS
C1	Sites Receiving Density from Heritage Buildings as per Section 42.3(5)(b).	Up to 5 F.A.R.	Up to 5 F.A.R.	<ul style="list-style-type: none"> o Available only in accordance with Council policy where density is being transferred from another site, as per Section 42.3(5)(h), or by purchase from a heritage preservation fund.
C2	Off-Site Improvements	Up to 3 F.A.R.	Up to 3 F.A.R.	<ul style="list-style-type: none"> • Available only in accordance with Council policy for a contribution to, or the construction of, features identified in Council's policy for public improvements in Downtown, and where such features are not located abutting the development site.
C3	<p>(a) Sites Receiving Density from the Density Transfer Area - Stephen (8th) Avenue Mall</p> <p>(b) Sites Receiving Density as a result of the protection of sunlight on Public Spaces</p>	Up to a maximum of 3 F.A.R., except on sites with a net site area of 1812 square metres or less where this limit may be exceeded.	Up to a maximum of 3 F.A.R., except on sites with a net site area of 1812 square metres or less where this limit may be exceeded.	<ul style="list-style-type: none"> • Available only in accordance with Council policy where density is being transferred from another site, as per Section 42.3(5)(i). o Available only in accordance with Section 4.23(5)(i).
C4	At-Grade Plaza – Additional (Excludes any Standard A1 and A3 spaces)	Where the additional plaza space is part of a plazas 250 square metres to 499 square metres in size - 10:1 Where the additional plaza space is part of a plazas 500 square metres or over in size - 15:1 Only the portion of plaza which exceeds 20 percent of gross site area is eligible for bonus density.	<ul style="list-style-type: none"> o At-grade or within 450 millimetres above or below grade with no wall along the sidewalk higher than 450 millimetres. o Direct access to sidewalk or on-site pedestrian space amounting to at least 50 percent of each of the plaza's street and avenue frontage. o Retail or personal service units abutting a plaza shall front onto it and have individual direct plaza level access to it. 	<ul style="list-style-type: none"> • Open to sky. • A minimum contiguous area of 250 square metres. • A minimum dimension of 9 metres in all directions. • A minimum frontage along street or avenue of 9 metres, with that frontage equal to or greater than the plaza depth. • A minimum ratio of 1 seat or 750 millimetres of bench seating for each 10 square metres of plaza area.

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS	LOCATION AND ACCESS REQUIREMENTS	PERFORMANCE REQUIREMENTS
C5	Exceptional Design	Up to: 1.0 F.A.R.		<p>Exceptional design includes a built form that:</p> <ul style="list-style-type: none"> (a) provides in terms of massing, orientation and façade design a positive contribution to the public realm, pedestrian environment and streetscape; (b) improves the pedestrian microclimate in terms of wind mitigation and/or sunlight penetration; (c) contributes positively to the vibrancy and activity of the urban environment at grade; (d) creates a sense of place and serves as a landmark in the urban environment; (e) employs materials, forms or building methods not commonly applied in the CM-2 district.
C6	+15 Bridge Refurbishment	22.5:1 Calculated on bridge floor area over right-of-way.		<p>The +15 Bridge Refurbishment must include upgrades to:</p> <ul style="list-style-type: none"> • Skylit: a minimum of 50% of the bridge must be skylit • Glazing: clear double glazed units over 75% of the bridge facade • Cladding: using light coloured material • HVAC: upgrades to system within the bridge where necessary to ensure the environment requirements of the +15 Policy • Flooring – upgraded to enhance long term maintenance • Decoration – internal painting to enhance pedestrian experience
C7	Heritage Based Public Art	1 square metre of floor area for every \$110 (October 1984 dollars) of sculpture provided. Maximum of 1 F.A.R.	<ul style="list-style-type: none"> o Setting acceptable to Approving Authority on site. o Minimum of 75 percent of total value contributed to Heritage Based Public Art used outdoors at-grade within a plaza space or on building exterior visible from the sidewalk. o Maximum of 25 percent of total value contributed to Heritage Based Public Art sculpture used in publicly accessible internal areas. 	<ul style="list-style-type: none"> • Unique not mass-produced sculpture by a practitioner in the visual arts who is generally recognized by critics and peers as a professional of serious intent and ability. • Value of the feature provided calculated in accordance with Council's approved policy. • Any internal must be located in an area within a public easement agreement.
C8	Heagle Façade	Up to: 0.5 FAR		<p>To be eligible for this incentive, the costs associated with the following restoration work will be covered by the developer:</p> <ul style="list-style-type: none"> • Removal, repair and storage of façade elements. • Cataloguing of all salvaged stones and windows. • Relocation of all salvaged materials (or storage until an appropriate location can be found).

STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS	LOCATION AND ACCESS REQUIREMENTS	PERFORMANCE REQUIREMENTS
C9	Urban Grove	8:1	a location visible from the public sidewalk or an on-site pedestrian space;	<p>An urban grove includes the following:</p> <ul style="list-style-type: none"> • a minimum of 10.0 trees, a minimum of 60.0 per cent of which are to be of the same species; • trees of a species capable of healthy growth in Calgary that must conform to the standards of the Canadian Nursery Landscape Association; • trees with an average 25-year life span; • 9.0 square metres of planting area for each tree planted; • a maximum of 25.0 per cent coniferous trees of all trees provided; • a minimum calliper size of 85.0 millimetres at the time of planting for deciduous trees; • a minimum height of 3.0 metres at the time of planting for coniferous trees; • a location within 0.45 metres above the grade of the adjoining public sidewalk where the urban grove is provided in a raised bed; and • an underground irrigation system.
C10	Additional Bicycle Parking Stalls	3.5:1 Up to a maximum of 1.0 F.A.R		<p>Additional bicycle parking stalls include the following:</p> <ul style="list-style-type: none"> (a) bicycle parking stalls – class 1 located either within the building or in a separate structure on the site; (b) a change room located either within the building or in a separate structure on the site with a minimum area of 30.0 square metres that contains: <ul style="list-style-type: none"> (i) 1.0 locker for every 4.0 bicycle parking stalls – class 1; (ii) 1.0 shower for every 4.0 bicycle parking stalls – class 1; (iii) 1.0 grooming station consisting of wash basin, mirror and electrical outlet for every 4.0 bicycle parking stalls – class 1; (c) use only by the tenants of the development; (d) located with the minimum required bicycle parking stalls – class 1;



STANDARD	PUBLIC AMENITY TO BE PROVIDED	BONUS	LOCATION AND ACCESS REQUIREMENTS	PERFORMANCE REQUIREMENTS
C11	Bicycle Facilities	7.5.1 up to a maximum of 2.0 FAR	<p>where located in a parkade:</p> <ul style="list-style-type: none"> (i) must be on the closest parkade level to grade and physically separated from the motor vehicle parking stalls; (ii) must have bicycle lanes within parkade ramps where these are shared with motor vehicles. 	<p>A bicycle station includes the following:</p> <ul style="list-style-type: none"> (a) bicycle parking stalls – class 1 that are not required bicycle parking stalls – class 1 located either within the building or in a separate structure on the site; (b) a change room located either within the building or in a separate structure on the site containing a minimum of the following facilities: <ul style="list-style-type: none"> (i) 1.0 shower for every 10.0 bicycle parking stalls – class 1 greater than the minimum requirement for the first 200.0 bicycle parking stalls – class 1; and (ii) 1.0 grooming station consisting of wash basin, mirror and electrical outlet for every 10.0 bicycle parking stalls – class 1 greater than the minimum requirement for the first 200.0 bicycle parking stalls – class 1; and (iii) 1.0 toilet for every 25.0 bicycle parking stalls – class 1 greater than the minimum requirement for the first 100.0 bicycle parking stalls – class 1; and (iv) 1.0 locker for every 4.0 bicycle parking stalls – class 1; (f) use only by the tenants of the development; (c) a bicycle repair space of at minimum 2.0 metres by 3.0 metres in dimension.

